

202 Trail (Central Park to Route 313) Planning and Feasibility Study

FINAL REPORT

*Doylestown Township
Doylestown Borough
Bucks County, PA*

July 2021
Prepared for:

Doylestown Township



Doylestown Borough



Acknowledgements:

Doylestown Township, Doylestown Borough and Michael Baker International would like to thank the Delaware Valley Regional Planning Commission / Regional Trails Fund for supplying the grant funding for this study. We would also like to thank the William Penn Foundation for their generous support of this program.



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1) Introduction to Study Area

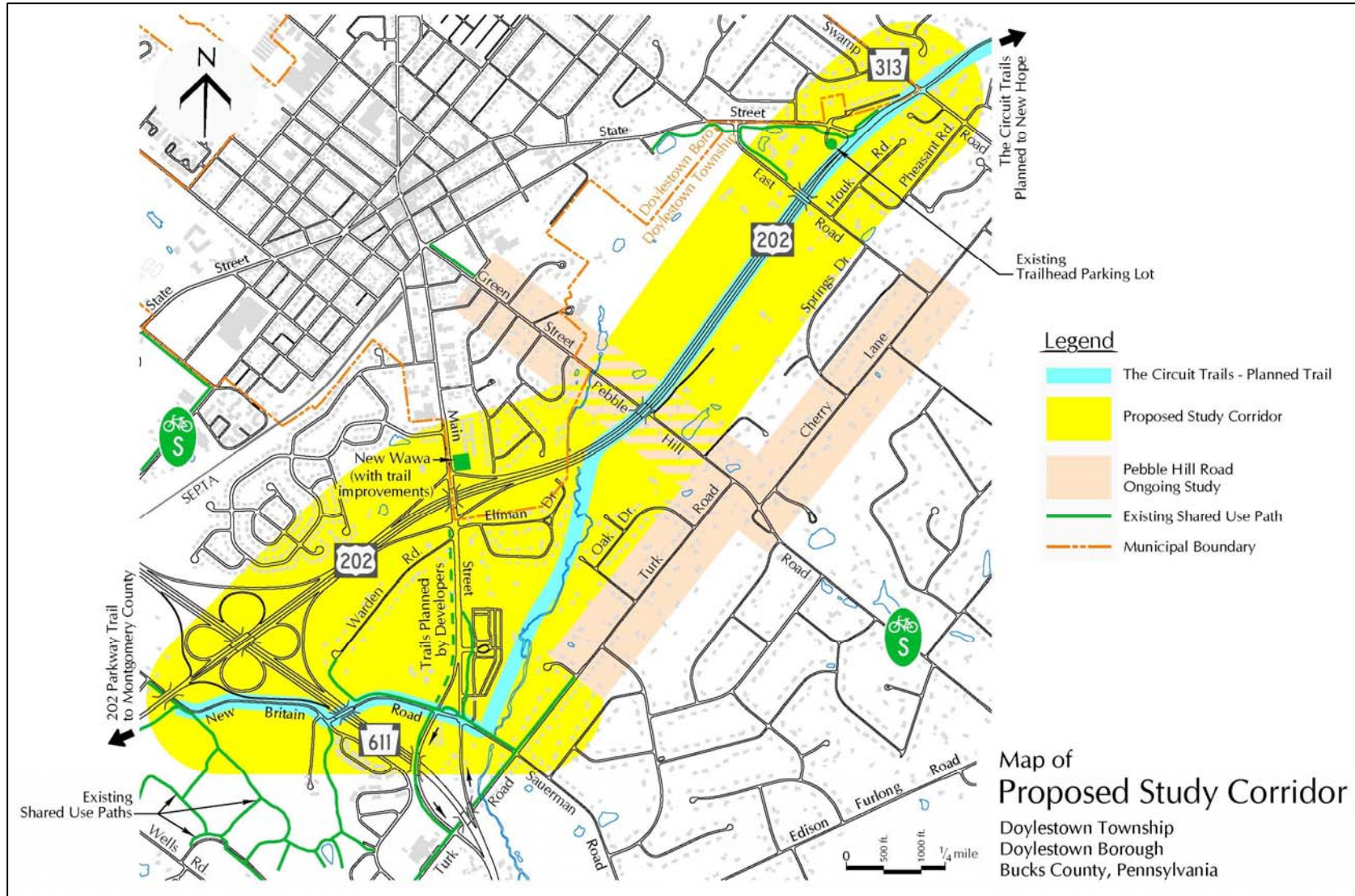
The Doylestown Community Bike and Hike Committee has a goal to connect their communities to the major natural, cultural, and commercial resources in the area through a comprehensive walking and bicycling trail network. For several years, Bucks County, the Circuit Coalition and other stakeholders have been discussing the concept of a trail facility along the Route 202 corridor between Doylestown and New Hope. This route was part of the original Doylestown Community Bike & Hike plan envisioned over 25 years ago and is aimed at connecting residential neighborhoods to the major places of employment in Central Bucks including the Doylestown Hospital, Central Bucks School District, and County of Bucks governmental offices. All of which are within pedestrian / bicycle commuting distance of the proposed trail.



This planning and feasibility study will help advance completion of a critical section of the 202 Trail between Doylestown Borough/Township and Buckingham Township. This 2.8-mile-long project will help connect the 30+ mile Doylestown Community Bike and Hike network to Buckingham Township and extend the very popular 202 Parkway Trail. This project will also coordinate the planning of several on-going trail related initiatives in this area including:

- Trail improvements and financial contributions to the design of future trails resulting from a proposed Wawa development at the Easton Rd. interchange with Route 202 Bypass
- Trail construction on the west side of Easton Road approved through several private development projects
- Ongoing planning efforts from the Turk Road / Cherry Lane Neighborhood Bicycle/Pedestrian Trail Feasibility Study (funded by DCNR). This study is often referred to as the “Turk Rd. Trail Study.”
- Recent trailhead construction and shared use path construction as part of PennDOT’s SR 202/SR313 Pools Corner roadway project.
- Planned redevelopment on the 50+ acre Tabor Services property at New Britain/Easton Rd. which will likely lead to approx. 1 mile of new trails
- Extending the existing 202 Parkway Trail

The study area includes Doylestown Borough and Doylestown Township and is generally centered on the Route 202 Bypass. The study area starts at the north end of the 202 Parkway trail in Doylestown's Central Park and extends north to the intersection of Route 202/SR 313. One of the region's highest quality trails, the 202 Parkway Trail, ends at the south end of this study area at New Britain Road. Extending the 202 Parkway Trail by almost 3 miles will be one of the main goals of the study.



2) Scope of Study

The scope of this study includes the following major tasks:

- Right of Way Research Along the Route 202 Bypass and Study Corridor
- Field Views to Identify Constraints and Opportunities
- Environmental Constraints and Cultural Resources
- Opportunities and Constraints Identification
- Trail Alternatives Development and Analysis
- Public Outreach Activities/Public Meeting
- Development of Preferred Alternative and Concept Plan
- Project Cost Estimate for Preferred Alternative
- Implementation Plan for Trail Development
- Final Study Report

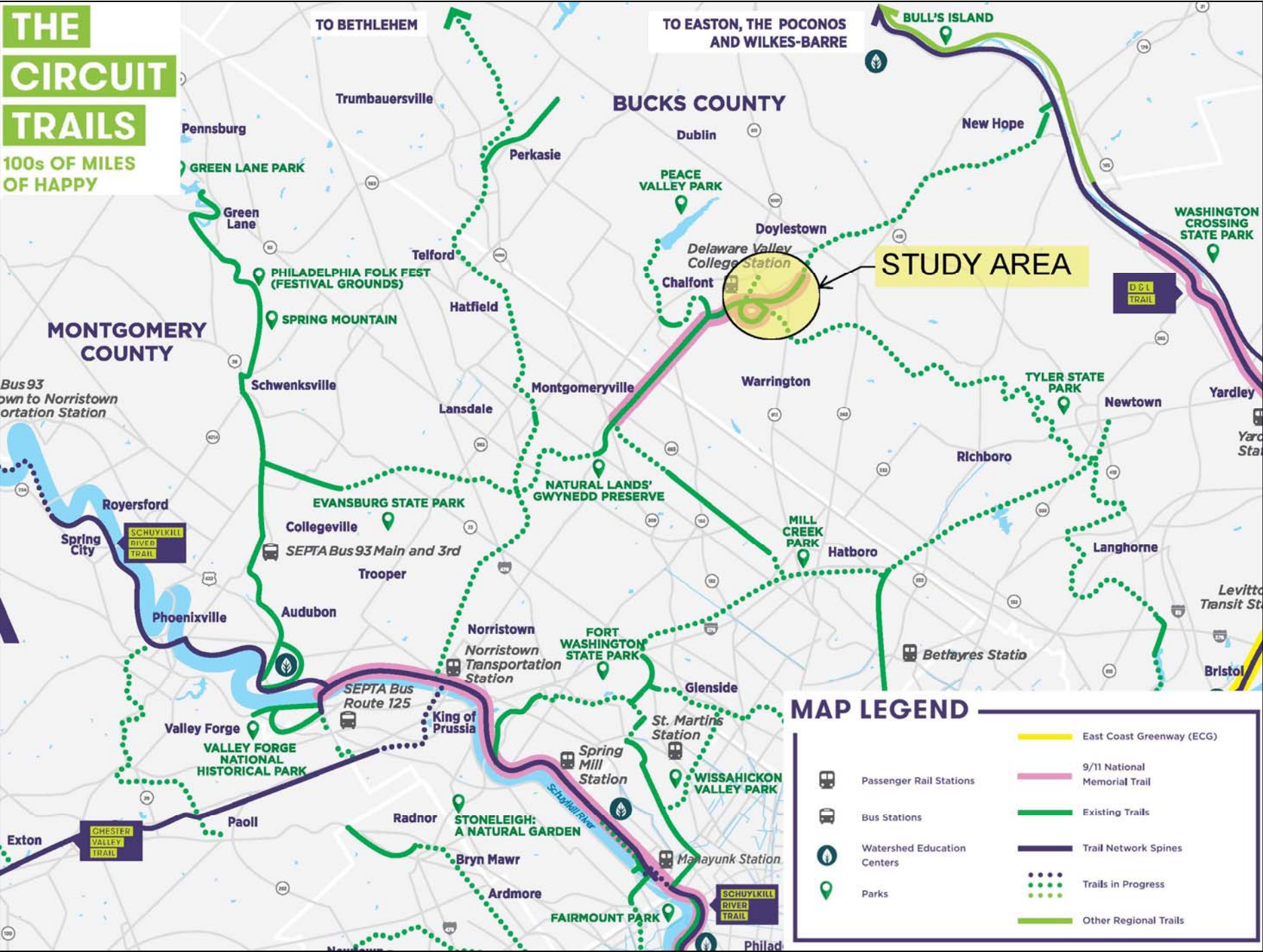
3) Regional Context

Doylestown is the County seat of Bucks County. The courthouse and other County offices create a great deal of business and commercial activity in the Borough of Doylestown. The Borough is also a major destination of the SEPTA Lansdale/Doylestown commuter line that extends to center city Philadelphia. Doylestown Borough is also a main street village that is centered on the major roadway of US Route 202. Doylestown Township is more rural and has become a popular area for residential development outside the Borough and along US 202 and the SEPTA rail line. The study area is located within the Circuit Trails network. The proposed trail will be an extension of the existing 202 Parkway Trail and part of the planned trail connection between Doylestown and New Hope. This study area also connects to the Neshaminy Creek Greenway Trail which is the major spine of the trail network within Bucks County. This proposed trail will enhance connections to both the 202 Parkway Trail, the Neshaminy Trail as well as the 30 mile Doylestown Community Bike and Hike network.

This study will continue the goal for enhanced connectivity to the regional Circuit Trail Network. The map on the following page shows the context of this study in relation to the large regional Circuit Trails network.

THE CIRCUIT TRAILS

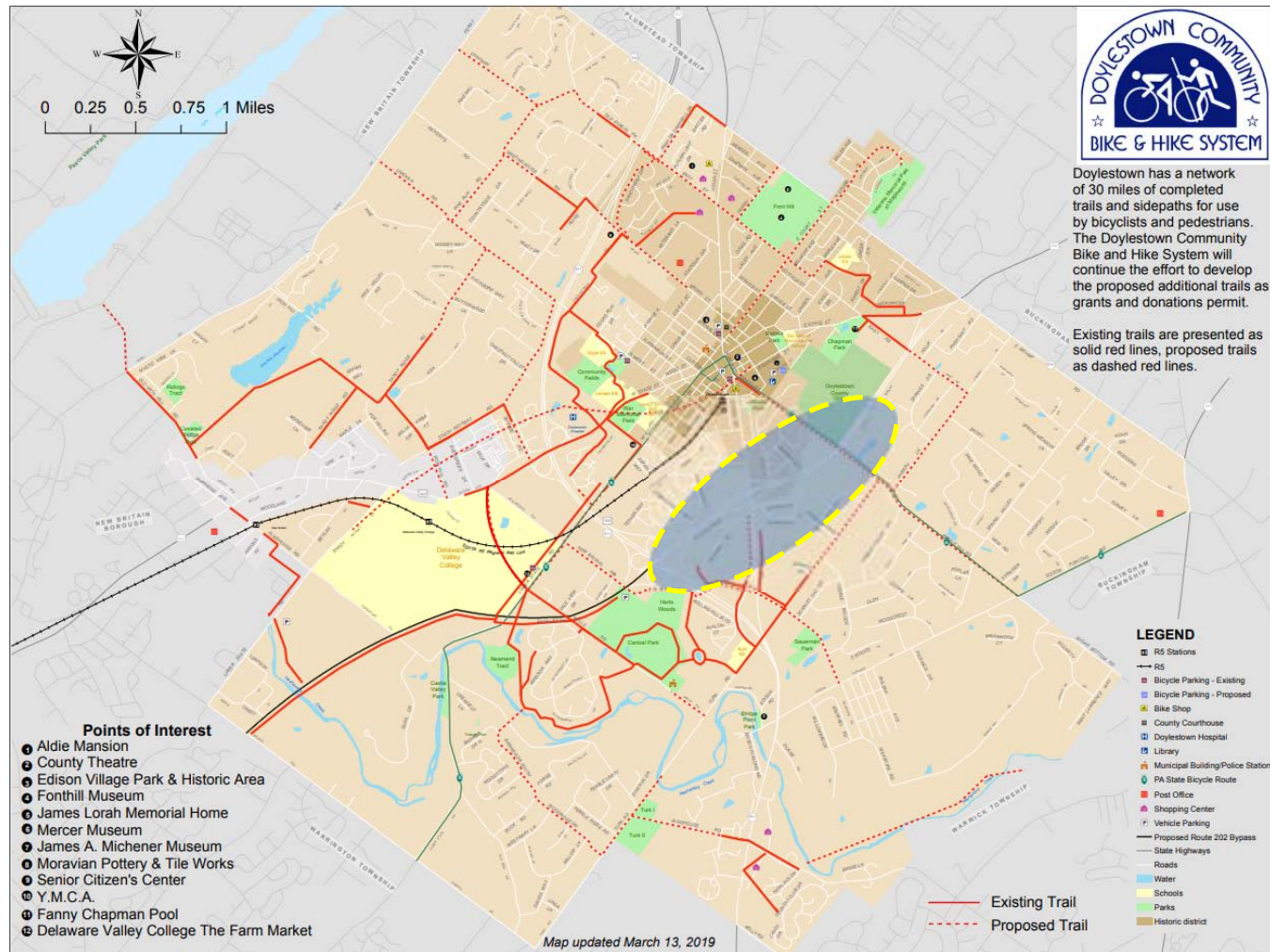
100s OF MILES OF HAPPY



MAP LEGEND

	East Coast Greenway (ECG)
	9/11 National Memorial Trail
	Existing Trails
	Trails in Progress
	Trail Network Spines
	Parks
	Watershed Education Centers
	Bus Stations
	Passenger Rail Stations
	Parks
	Watershed Education Centers
	Parks
	Parks
	Parks

The Doylestown Community Bike and Hike Committee has been in existence for almost 30 years. They have been working diligently over that period



Doylestown Community Bike and Hike Trail Map with Study in highlight oval.

to develop a community wide system of walking and biking trails to benefit the area. Doylestown Borough and Doylestown Township together, in 1992, formed a joint committee to study the concept of developing a multi-use path that would surround the Borough. The initial plan showed trails along Turk Road, Cherry Lane, and Pebble Hill Road/Green Street.

The committee began meeting in October of 1992. The first committee meetings tackled the overall concept of the initial plan and developed a list of community resources that should be connected to the bike and hike system. The Community has been very successful working with private development projects to augment the system. In addition, they have successfully acquired several grants from PennDOT to make major additions to the system. In 2005, the formal bike and hike map (pictured left) was created through a grant

from the Delaware Valley Regional Planning Commission and it's been kept current throughout the years. To date, the Committee has developed a system of over 30 miles of trails throughout the community.

In 2011 and 2012, Bucks County adopted the Bucks County Open Space & Greenways Plan (2011) and the Bucks County Bicycle Master Plan (2012). These plans identified that Bucks County contains:

- 350 miles of bicycling facilities
- 282 miles of shared-use trails
- 72 miles of hiking trails and nature paths
- 48 miles of equestrian trails

Easton Road from Taber Property to Doylestown Borough:

There are several on-going public and private projects in the project area that will construction trails or other improvements that will benefit the development of the proposed 202 to 313 Trail. Progressing along Easton Road from south to north the projects are as follows:

- Tabor Land Development (601 New Britain Rd)
- Callan Tract (2140 Easton Road)
- Wawa Project (425 S Main St, Doylestown)
- South Main Street Trail (SR 202 Ramp to Steeplechase Drive)

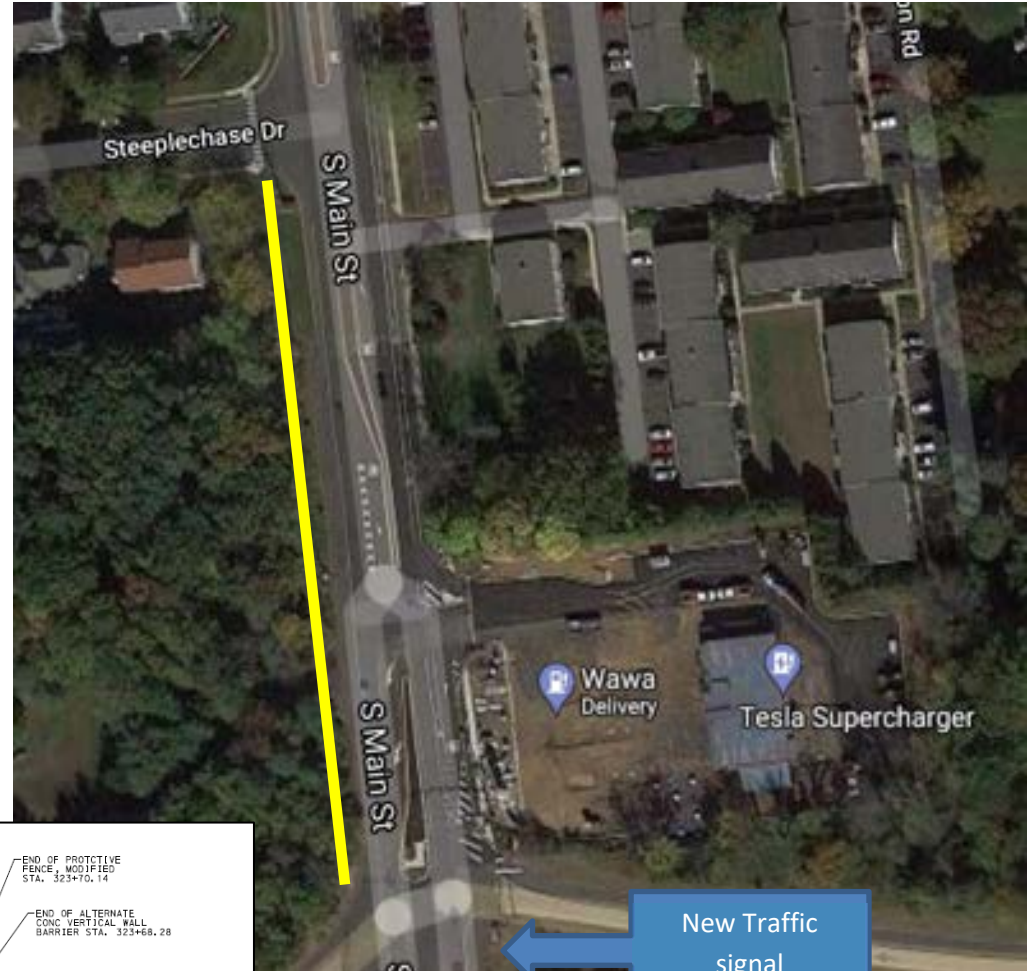


Callan Tract (2140 Easton Road)

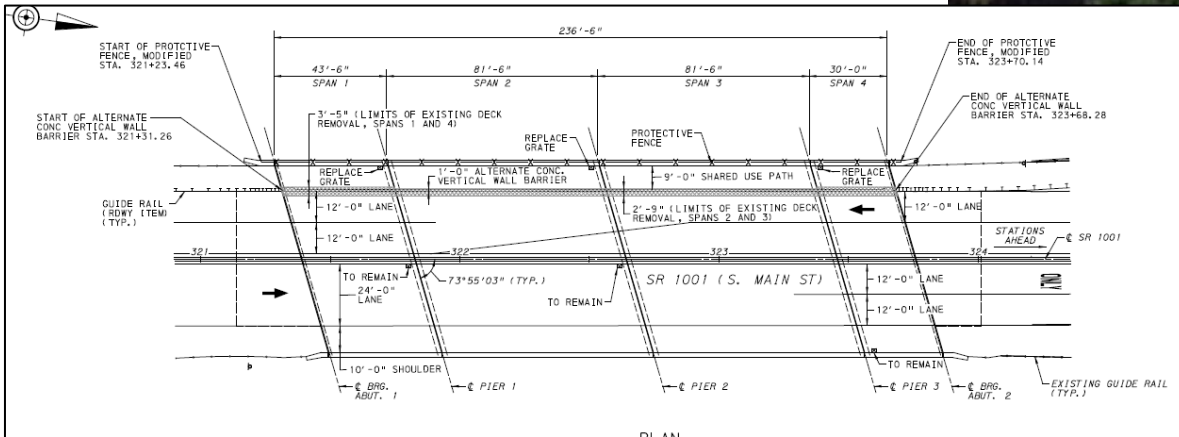
In 2017, an additional land development plan was submitted to Doylestown just north of the Tabor project. A shared use path was requested along the frontage of that property as well. If that project moves forward, an additional 260' of trail will be constructed by a development project along the west side of Easton Road.

Wawa Project (425 South Main Street)

A Wawa gas station/ and food store was constructed last year at the northwest corner of the Easton Road / Route 202 interchange. Wawa installed a new traffic signal, bike lane into the Borough crosswalk across Easton Road at the ramp location. Wawa also made a funding contribution toward the design and construction of a shared use path connection from Steeplechase Drive to the SB on ramp to Route 202 (shown in yellow). Wawa also provided conceptual design of a structure mounted barrier along the shoulder of the Easton Road bridge to accommodate a protected trail area in the future. See Appendix for those plans.

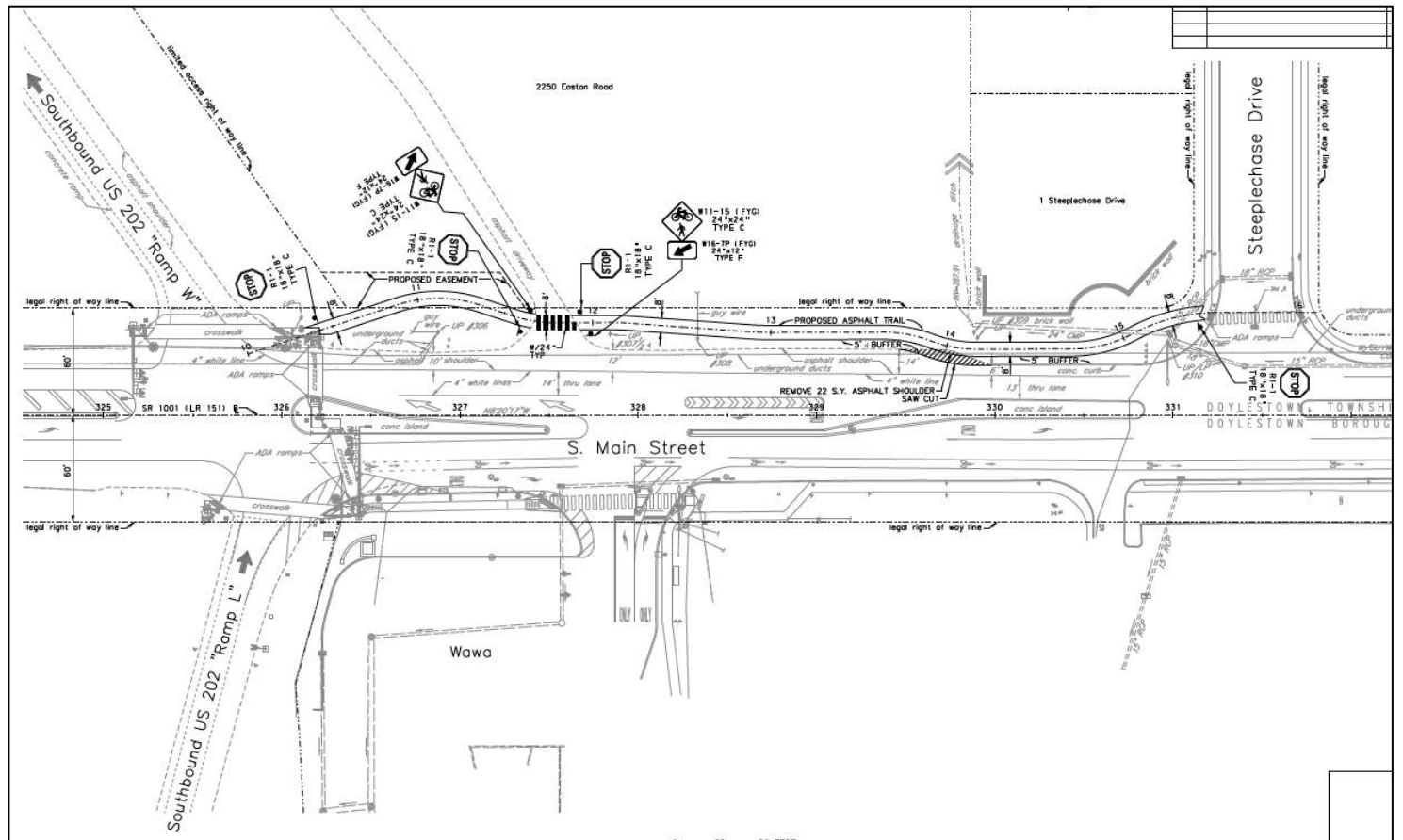


New Traffic signal



South Main Street Trail Project

As mentioned above, the Wawa project contributed toward the design and construction of a connector trail between the 202 ramps and Steeplechase Drive. Michael Baker designed this 600' long section of trail and acquired approval from PennDOT District 6-0 to construct the trail within the legal right of way of South Main Street/Easton Road. A trail easement was also acquired from the Bucks County Historic society to properly design the trail and eliminate major utility impacts. The trail is anticipated to be construction by Doylestown Twp and Borough Public Works staff in 2022. See the Appendix for a copy of these plans.



4) Trail Alternatives Development and Analysis

Analysis of aerial photography, background research and field views were conducted to determine the major terrain features, adjacent property uses and environmental issues that would affect the construction of this trail facility. The corridor was evaluated to determine the most appropriate trail facility and most feasible approach for its construction. According to AASHTO's Guide for the Development of Bicycle Facilities, there are 4 different types of bicycle facilities. The selection of the facility should be based on factors such as the ability of the users, specific corridor conditions, and facility cost. The different types are the following:

Shared Roadway (No Bikeway Designation): This facility represents the majority of existing bicycle travel in the United States. Some street systems provide efficient bicycle travel and do not require the use of additional signing and striping. In other instances, it would be inappropriate to designate a certain route a bikeway because the roadways are not suitable for bicycling. In some cases, such as residential areas, the bicycle demand is not high enough to warrant a bikeway designation. Rural highways that are used for intercity and recreational travel should only be designated as a bikeway where there is a need for enhanced continuity with other bicycle routes.

Signed/Marked Shared Roadway: This facility is designated by bike route signs, Bikes May Use Full Lane signs and/or shared lane markings (i.e. sharrows) along an existing roadway and has two purposes. One purpose is to provide continuity to other bicycle facilities, such as bike lanes. Another purpose is to designate preferred routes through high-demand corridors. These routes are implied to be more advantageous than alternate routes to bicyclists. Therefore, responsible agencies should ensure that these routes are adequate and well maintained for the needs of bicyclists. The special signing and pavement markings also makes drivers aware of the presence of bicyclists.



Example of a Shared Roadway using shared lane markings and signage. Source: NACTO



As indicated in the NACTO guide, shared Lane Markings (SLMs), or “sharrows,” are road markings used to indicate a shared lane environment for bicycles and automobiles. Among other benefits shared lane markings reinforce the legitimacy of bicycle traffic on the street, recommend proper bicyclist positioning, and may be configured to offer directional and wayfinding guidance. The shared lane marking is a pavement marking with a variety of uses to support a complete bikeway network; it is not a facility type and should not be considered a substitute for bike lanes, cycle tracks, or other separation treatments where these types of facilities are otherwise warranted or space permits. The MUTCD outlines guidance for shared lane markings in section 9C.07.

Bicycle Boulevards:

A Bicycle Boulevard is a road or street that is ideal for bicyclist transport through specialized road treatment such as traffic calming and speed reduction, way finding signage and pavement markings, and intersection crossing treatments. The following identifies some of the specific treatments that can be used to create bicycle boulevards:

Signage

- Identification Signs
- Way-finding Signs
- Warning Signs
-

Prioritize Bicycle Travel on Bicycle Boulevard

- Pavement Markings
- Stop/Yield Signs

Intersection Treatment

- Bicycle Boxes/Advanced Stop Bar
- Bicycle Activated Signals
- High Visibility Raised Crossing Islands

Traffic Calming

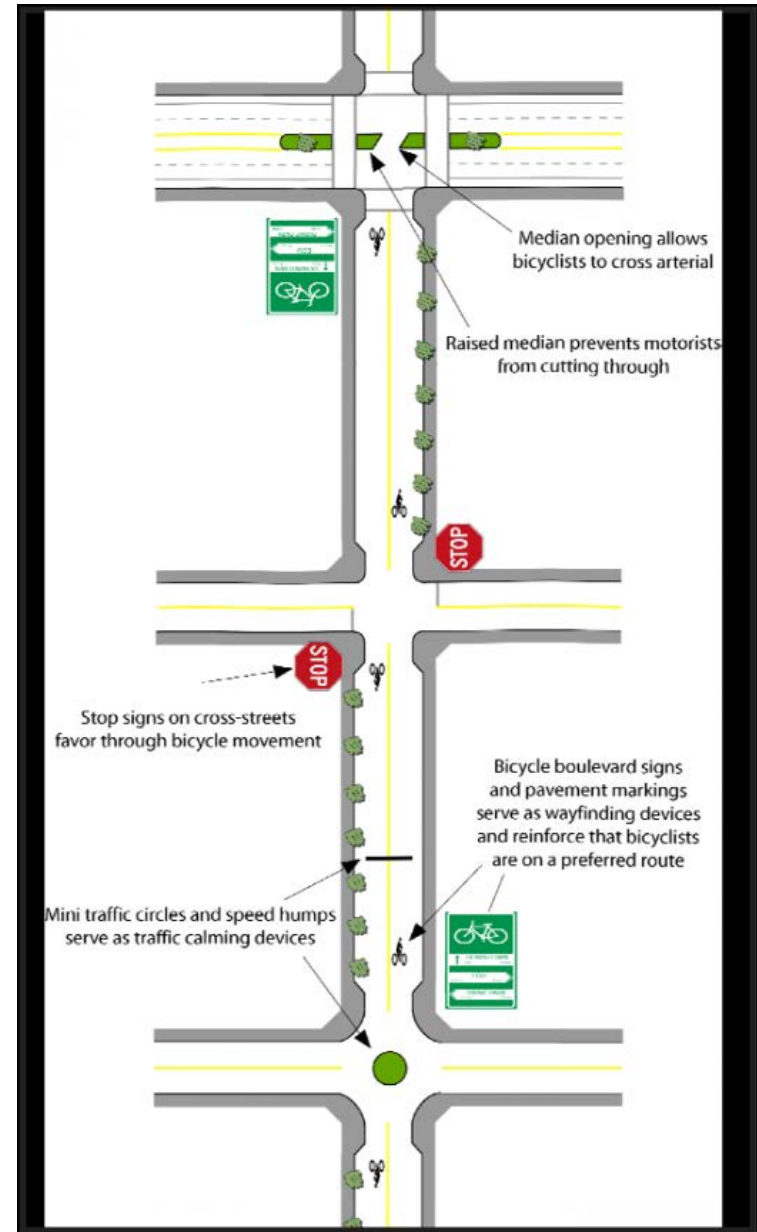
- Roundabouts
- Speed Tables
- Painted and Patterned Surfaces
- Chicanes
- Curb Extensions
- Residential Speed Limit
- Advisory Bicycle Lane
- Contraflow Bicycle Lane

Traffic Reduction

- Non-Motorized Only Crossings
- Partial Non-Motorized Only Crossings



A combination of the above treatments is often utilized to enhance the use of specific roadways as bicycle boulevards. The above treatments are typically used on a regular interval along a number of blocks of a roadway corridor to as shown in the figures below.





Bicycle Lane: This facility's purpose is to enhance conditions for bicyclists on the streets through the use of appropriate pavement markings and signing. Bike lanes are used in areas where there is both a high bicycle demand and distinct needs that can be served by using them. Bike lanes delineate the right-of-way for bicyclists and motorists, separating the traffic and increasing the capacity of highways with mixed traffic. Bike lanes also provide an adequate area for bicyclists where there is insufficient space on the roadway for comfortable riding. For effective bike lanes, bicycle-safe drainage inlet grates should be used, pavement surfaces should be smooth, and traffic signals should be responsive to bicyclists. Regular maintenance should be a top priority to ensure potholes, broken glass, debris, or other impediments do not interfere with the bicyclists.

Example of Bike Lane

Shared Use Path: This facility consists of any independent trail on a separate alignment specifically designed for pedestrians and bicyclists. Shared use paths (SUP) are often constructed along rivers, ocean fronts, canals, utility rights-of-way, former or active railroad rights-of-way, within college campuses, within and between parks, or as part of a planned development. Shared use paths offer opportunities not provided by road systems, such as recreation or a direct commute. A shared use path is designed with the safety of all users in mind. This includes bicyclists, joggers, pedestrians, dog walkers, people with baby strollers, people with disabilities, roller bladders, etc.



Example of Shared Use Path

It is important that the proposed facility does not encourage or require bicyclists or motorists to operate in a manner that is different from the rules of the road. The needs of both bicyclists and motorists must be considered in the selection of the facility. Continuity of the overall system should be taken into consideration in the selection of the facility. Alternating segments of shared use path and bike lanes may result in street crossings at the end of the segments or wrong-way bicycle travel beyond the limits of the path due to the inconvenience of crossing the street. Sidewalks should be used in limited circumstances, such as along bridges or in areas of sporadic bicycle use. Any considerable difference in height between the sidewalk and roadway should be protected by a suitable barrier.

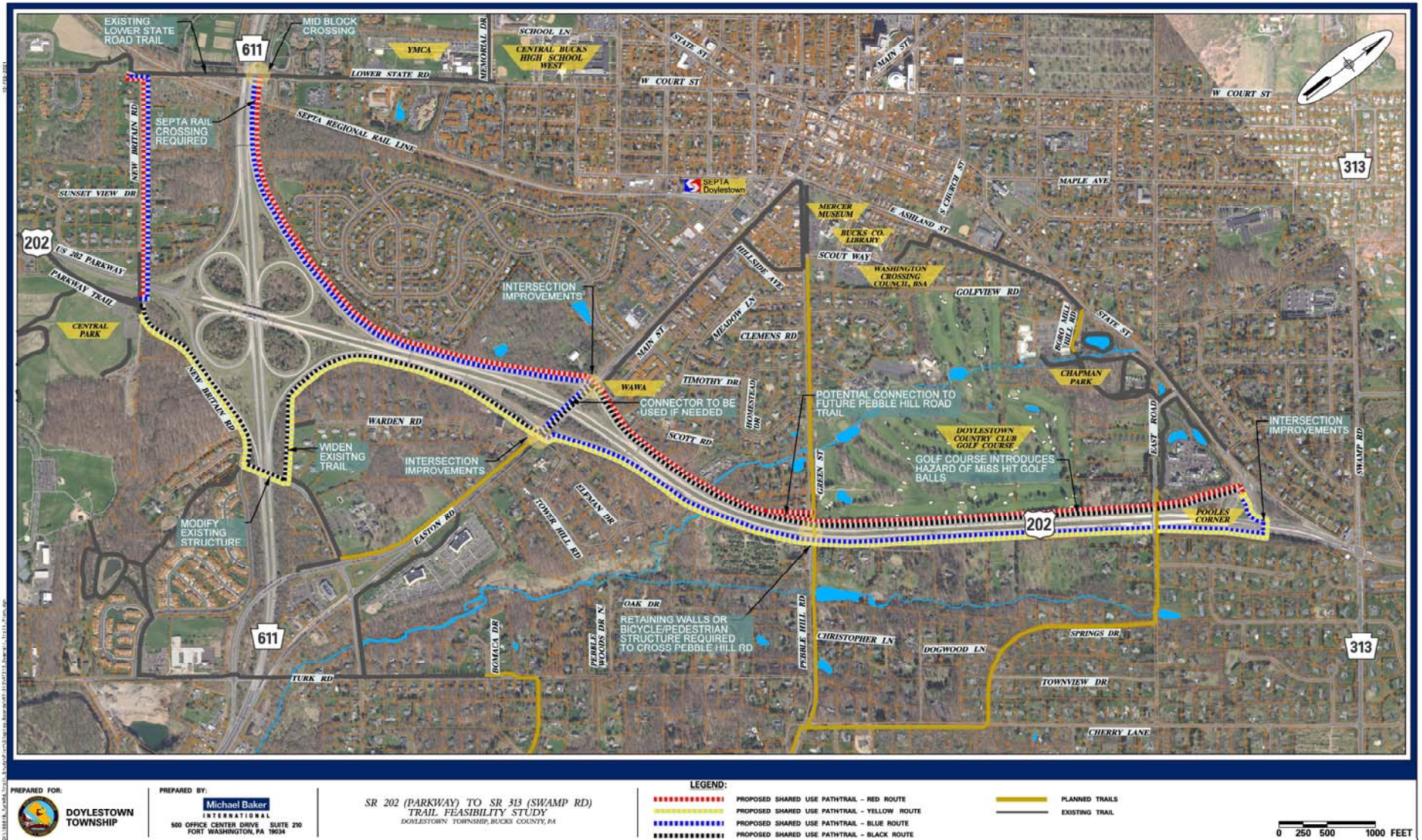
Bicycle/Pedestrian Bridges and Crossings: Crossings of waterways, roadways and other obstacles can be some of the most difficult and costly portions of a trail project due to design, permitting and construction issues. Crossings of waterways can be complicated by the need for various types of permits from state Departments of Environmental Protection or other agencies. These permits can vary from a minor application showing a sketch of the crossing to permits that required in-depth hydraulic models, reports and coordination with the Federal Emergency Management Agency (FEMA) to determine flood plain impacts. The following are several methods that are typically used on trail projects for waterway crossings: ford, low flow pipe culverts, metal/concrete box or arch culvert, pre-fabricated bridge (various materials) and custom designed bridge.



Example of Independent Trail Bridge

Trail Alternative Analysis:

The above trail facility and structure types were considered in this initial feasibility study. Four alternative routes have been investigated to connect the 202 Parkway Trail at the north end of Central Park to Route 313 at Poole's Corner. The four alternatives are color coded and shown on the map below. The routes generally follow the Route 202 Bypass within the limited access right of way on the east side or the west side. See Appendix for full plan.



Red Route: Starting at the north end of Central Park/Walt Berry Trailhead at the end of the 202 Parkway Trail, this alternative would proceed west on the existing side path trail to Sunset View Drive. The proposed trail would continue on the south side of New Britain road for approximately 1200' to the new development at Oxford Lane. Near Oxford land, a new pedestrian bridge would be required to cross a small creek. The trail would cross over New Britain Road to the north side to take advantage of a new trail that will be constructed as part of a private development. A future development is planned at the northeast corner of New Britain Road/Lower State Road intersection that will include a shared use path along the frontage as well. Intersection improvements from the adjacent development will connect the proposed path to the existing Lower State Road Trail at the existing signalized intersection. This alternative will use the existing Lower State Road Trail to travel north for approximately 1300' to the north side of the 611 Bypass. A new mid-block crossing would be installed to reach the north side of the Bypass. The trail route will follow the 611 Bypass until intersecting with the SEPTA Lansdale Doylestown railroad line. A new railroad cross would be required which will be difficult to acquire since there is an existing road crossing with the railroad a short distance away. This trail route would continue east along the 611 Bypass and 202 Bypass to Easton Road.

The red route would use the existing signalized crosswalk to cross Easton Road to continue along the 202 Bypass ramp and mainline to Pebble Hill Road. A new trail bridge with a length of approximately 175' would span over Pebble Hill Road. A connection from the 202 Bypass Trail to the future planned trail along Pebble Hill Road would be recommended. The trail would follow the west side of the 202 Bypass along the Doylestown Country Club property, passing under the East Road bridge. Barrier or guiderail is anticipated to separate the trail and roadway where the trail passes under the bridge near the piers. The last segment of the trail would extend behind the existing stormwater basin to connect to the existing paths constructed around the Poole's corner trailhead/park and ride lot.



Looking west from 202 Trailhead along existing path to Sunset View Drive.

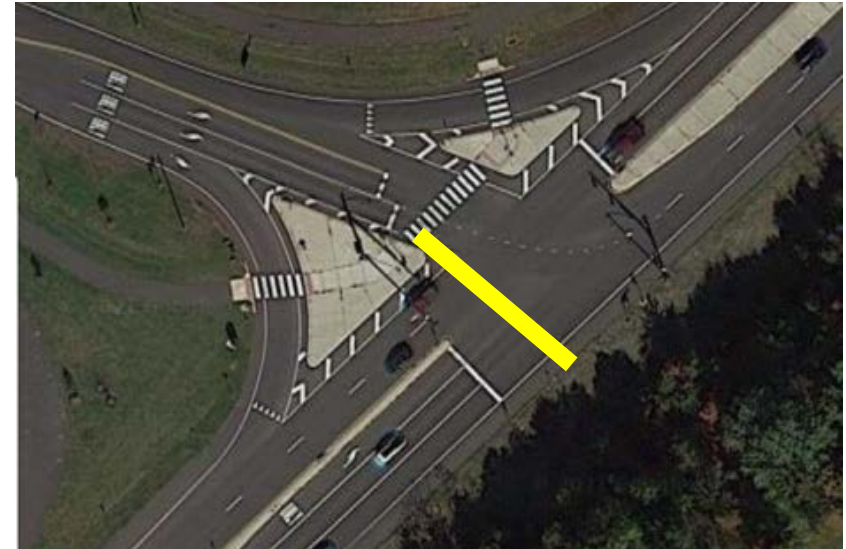
Advantages of this alternative:

- Connection to Sunset View Drive, New Britain Road and Oxford Lane neighborhoods
- Connection to existing Lower State Trail
- Utilizes existing traffic signal to cross Easton Road
- Potential connection to future trail along Pebble Hill Road
- No crossing of Route 202 at Poole's corner required
- No structure mounted barrier required on Easton Road bridge.

Disadvantages:

- Mid-block crossing of Lower State Road
- Likely require at least 1 residential trail easement at Lower State Road
- New crossing of SEPTA railroad – a major concern
- May require safety netting along Doylestown Country Club Golf Course

Blue Route: Starting at the north end of Central Park/Walt Berry Trailhead at the end of the 202 Parkway Trail, this alternative would proceed west on the existing side path trail to Sunset View Drive. The proposed trail would continue on the south side of New Britain road for approximately 1200' to the new development at Oxford Lane. Near Oxford land, a new pedestrian bridge would be required to cross a small creek. The trail would cross over New Britain Road to the north side to take advantage of a new trail that will be constructed as part of a private development. A future development is planned at the northeast corner of New Britain Road/Lower State Road intersection that will include a shared use path along the frontage as well. Intersection improvements from the adjacent development will connect the proposed path to the existing Lower State Road Trail at the existing signalized intersection. This alternative will use the existing Lower State Road Trail to travel north for approximately 1300' to the north side of the 611 Bypass. A new mid-block crossing would be installed to reach the north side of the Bypass. The trail route will follow the 611 Bypass until intersecting with the SEPTA Lansdale Doylestown railroad line. A new railroad cross would be required which will be difficult to acquire since there is an existing road crossing with the railroad a short distance away. This trail route would continue east along the 611 Bypass and 202 Bypass to Easton Road. This alternative would then turn south using the existing shoulder of the Easton Road bridge to reach the south side of the 202 Bypass. A structure mounted concrete barrier or other approved separation would be installed to safe separate the trail users from the vehicle lanes. Intersection improvements would be needed at the south side interchange ramps/Easton Road intersection to allow trail users to safely cross Easton Road. The trail route would parallel the south side of the 202 Bypass up to Pebble Hill Road. A new trail bridge with a length of approximately 175' would span over Pebble Hill Road. A connection from the 202 Bypass Trail to the future planned trail along Pebble Hill Road would be recommended. The trail would follow the east side of the 202 Bypass passing under the East Road bridge. The trail would extend from East Road to the existing traffic signal at East State Street where intersection improvements would provide a new crosswalk (shown in yellow in image above) to cross Route 202 to the existing trails at the Poole's corner parking area.



Existing traffic signal at Route 202 / East State Street at Poole's Corner with new crosswalk in yellow

Advantages of this alternative:

- Connection to Sunset View Drive, New Britain Road and Oxford Lane neighborhoods
- Connection to existing Lower State Trail
- Potential connection to future trail along Pebble Hill Road

Disadvantages:

- Mid-block crossing of Lower State Road
- Likely require at least 1 residential trail easement at Lower State Road
- New crossing of SEPTA railroad – a major concern
- Requires new crossing of Easton Rd at NB off ramp
- Steeper slopes on east side of the 202 Bypass
- Crossing of Route 202 at traffic signal at Poole's corner

Yellow Route: Starting at the north end of Central Park/Walt Berry Trailhead at the end of the 202 Parkway Trail, this alternative would proceed east along New Britain Road. The existing roadway includes 8-10' wide gravel shoulders on both sides. Relocation of the guiderail closer to the edge of road widening of the fill slope is anticipated to create space for a shared use path on the south side of the roadway. This configuration would extend to existing trail at the intersection of New Britain Road and Rolling Hill Blvd. This trail route will then cross over Rolling Hill Blvd to the New Britain Road bridge over the 611 Bypass. This alternative would run north using the existing shoulder of the New Britain Road bridge over to reach the north west corner of the bridge and the existing trail leading to Warden Road. The existing trail would be widened to 10' wide to meet Circuit Trail Network and AASHTO guidelines. The trail route will parallel the 202 Bypass within the limited access right of way up to Easton Road. Intersection improvements would be needed at the south side interchange ramps/Easton Road intersection to allow trail users to safely cross Easton Road. The trail route would parallel the south side of the 202 Bypass up to Pebble Hill Road. A new trail bridge with a length of approximately 175' would span over Pebble Hill Road. A connection from the 202 Bypass Trail to the future planned trail along Pebble Hill Road would be recommended. The trail would follow the east side of the 202 Bypass passing under the East Road bridge. The trail would extend from East Road to the existing traffic signal at East State Street where intersection improvements would provide a new crosswalk to cross Route 202 to the existing trails at the Poole's corner parking area.



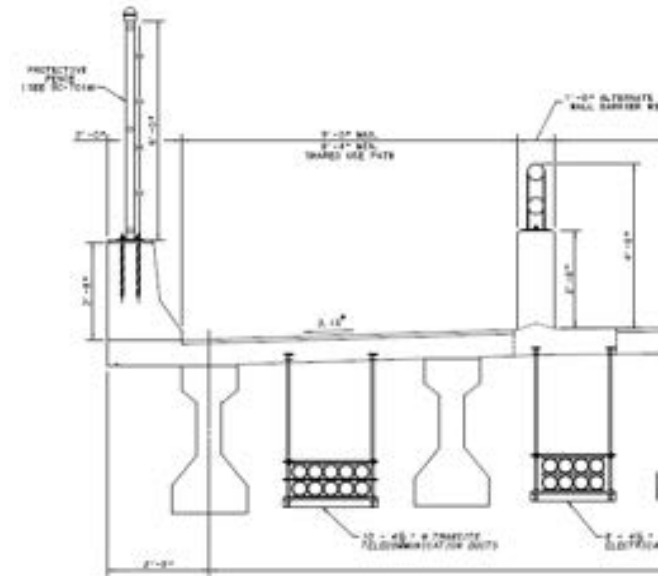
New Britain Road looking north with Route 611 Bypass ramp on left of photo.

Advantages of this alternative:

- Connection to Sunset View Drive, New Britain Road and Oxford Lane neighborhoods
- Connection/access to Heritage Conservancy Hart's Woods adjacent to New Britain Rd
- Potential connection to future trail along Pebble Hill Road

Disadvantages:

- Requires new crossing of Easton Rd at NB off ramp
- Steeper slopes on east side of the 202 Bypass
- Crossing of Route 202 at traffic signal at Poole's corner



Cross Section of Easton Road Structure Mounted Barrier for the shared use path

Black Route: Starting at the north end of Central Park/Walt Berry Trailhead at the end of the 202 Parkway Trail, this alternative would proceed east along New Britain Road. The existing roadway includes 8-10' wide gravel shoulders on both sides. Relocation of the guiderail closer to the edge of road widening of the fill slope is anticipated to create space for a shared use path on the south side of the roadway. This configuration would extend to existing trail at the intersection of New Britain Road and Rolling Hill Blvd. This trail route will then cross over Rolling Hill Blvd to the New Britain Road bridge over the 611 Bypass. This alternative would run north using the existing shoulder of the Easton Road bridge to reach the north west corner of the bridge and the existing trail leading to Warden Road. The existing trail would be widened to 10' wide to meet Circuit Trail Network and AASHTO guidelines. The trail route will parallel the 202 Bypass within the limited access right of way up to Easton Road. Intersection improvements would be completed at the intersection of the NB off ramp and Easton Road to allow trail users to access the west shoulder of the bridge. A structure mounted concrete barrier or other approved separation would be installed to safely separate the trail users from the vehicle lanes. Intersection improvements would be needed at the south side interchange ramps/Easton Road intersection to allow trail users to safely cross Easton Road. The trail route would parallel the south side of the 202 Bypass up to Pebble Hill Road. A new trail bridge with a length of approximately 175' would span over Pebble Hill Road. A connection from the 202 Bypass Trail to the future planned trail along Pebble Hill Road would be recommended. The trail would follow the east side of the 202 Bypass passing under the East Road bridge. The trail would extend from East Road to the existing traffic signal at East State Street where intersection improvements would provide a new crosswalk to cross Route 202 to the existing trails at the Poole's corner parking area.

Advantages of this alternative:

- Connection to Sunset View Drive, New Britain Road and Oxford Lane neighborhoods
- Connection/access to Heritage Conservancy Hart's Woods adjacent to New Britain Rd
- Utilizes existing traffic signal to cross Easton Road
- Potential connection to future trail along Pebble Hill Road
- No crossing of Route 202 at Poole's corner required

Disadvantages:

- May require safety netting along Doylestown Country Club Golf Course



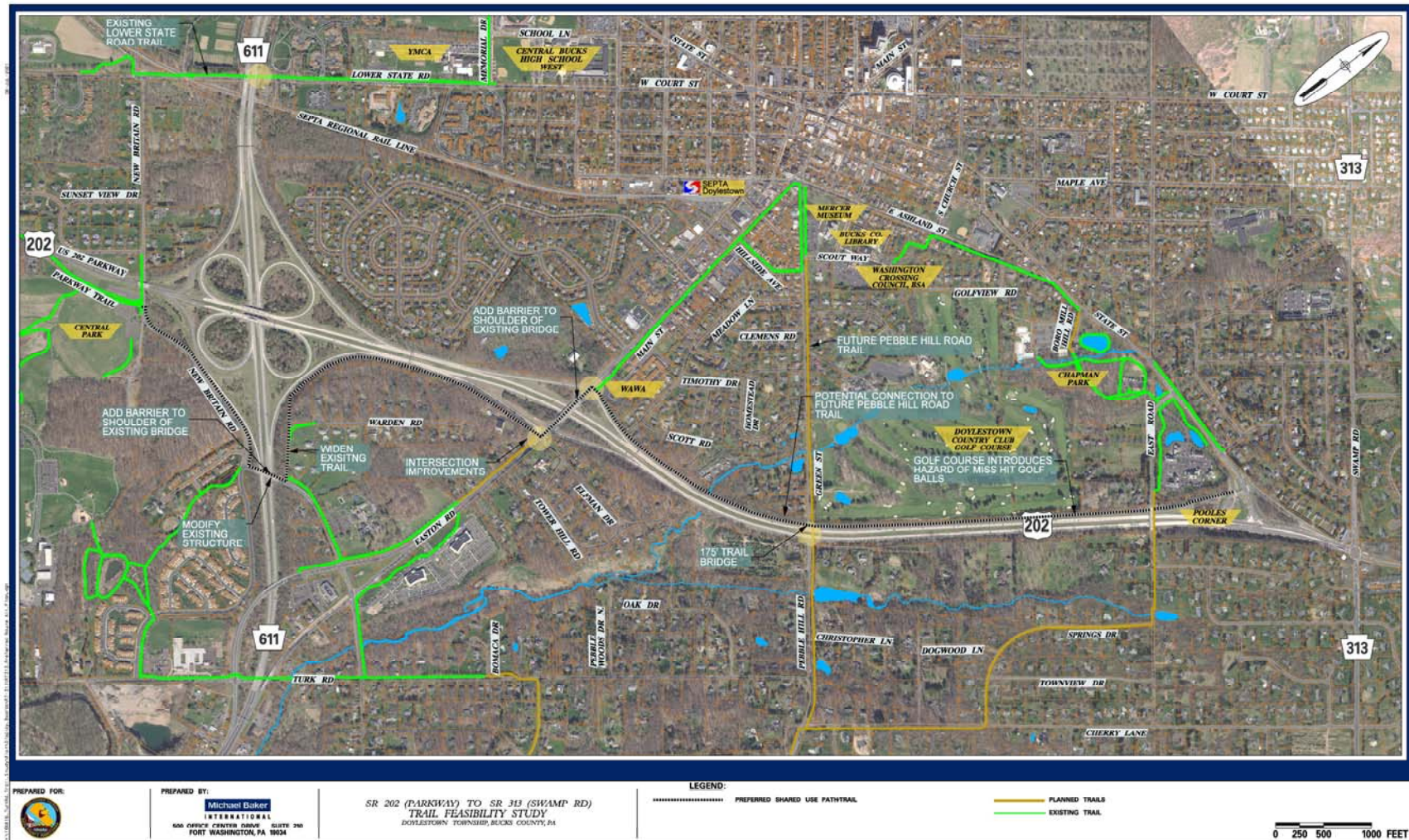
Existing New Britain Rd bridge over 611 Bypass (top) and Route 202 southbound with Doylestown Country Club golf course on right of photo (bottom).

Preferred Alternative:

The consensus of the steering committee, bike and hike committee and input from the public pointed to the Black Route being the preferred alternative. The reasons for this selection are the following:

- More advantages than disadvantages
- Little or no right of way acquisition required
- Very good connectivity to existing trails, future trails and neighborhoods
- Includes Easton Road structure mounted barrier which will assist with the future trail connection along Easton Road and uses the existing signal to cross Easton Road

See the Appendix for a full-size version of the preferred alternative plan as well as a plan showing the proposed trail on the existing roadway construction plans for the 202 Bypass.



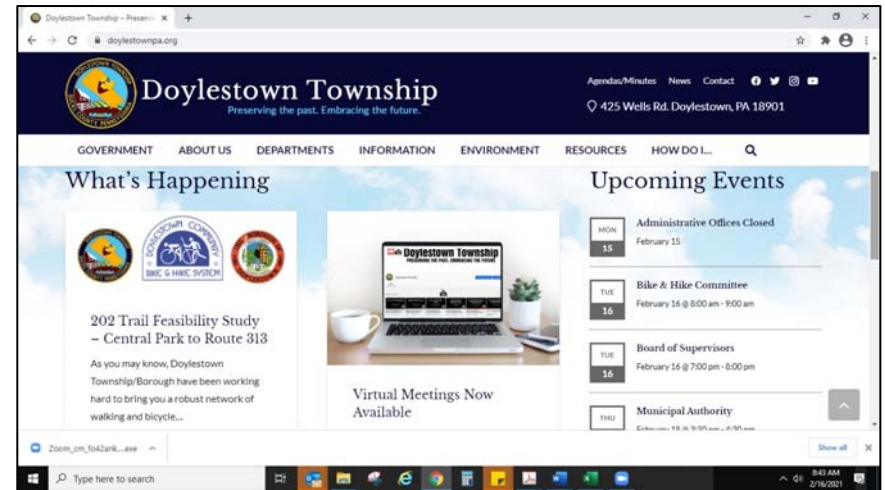
5) Public Participation Process

A comprehensive public outreach process has been utilized as part of this study including a study steering committee, regular discussions at the public Doylestown Community Bike and Hike meetings and the use of a virtual public meeting room website.

This effort has been directed by a steering committee made up of representatives of Doylestown Township, Doylestown Borough, and representatives of the Doylestown Community Bike and Hike Committee.

Doylestown Community Bike and Hike committee meetings were held monthly to move the project forward. This study is a standing agenda item for each meeting and progress was discussed at each. Questions, comments, and next steps were also discussed each month. These meetings are open to the public and are held at 8:00 am on the third Tuesday of each month.

Due to restrictions during the COVID-19 pandemic, a virtual public meeting room website was created to allow residents, stakeholders and other members of the general public the opportunity to review the study information, project alternatives and complete a survey. The survey allowed the public the opportunity to provide feedback and to make their vote for the trail alternative they thought was best for themselves and the community. Residents from the affected project area were contacted directly by letter (as shown to right) to inform them about the study and the virtual public meeting room website. The virtual public meeting site was also advertised on the Township website as shown above.



Doylestown Bike and Hike Committee Presents:
202 Trail Feasibility Study – (Central Park to Route 313)
Virtual Public Meeting

As you may know, Doylestown Township/Borough have been working hard to bring you a robust network of walking and bicycle trails for over 25 years. Recently, a \$318 grant was awarded through the Delaware Valley Regional Planning Commission's trails program for a feasibility study centered along Route 202 between Central Park and Route 313. This 2.8-mile-long project will help connect the 30+ mile Doylestown Community Bike and Hike network to Buckingham Township and extend the very popular 202 Parkway Trail. We invite you to attend a virtual public meeting to share your feedback on the progress of this project to date.

In a 3-D virtual setting, you will be able to:

- Review project information
- Explore alternative trail alignments
- Learn about anticipated opportunities and challenges of this project
- Share feedback to help guide this project towards a successful completion.

The virtual public meeting room will be hosted for a two-week time period on Doylestown Township and Borough's websites (Links Below) and we hope to receive your much-needed feedback to help progress this important link in the Bike and Hike network.

Date Range:	Sunday, February 7th to Sunday, February 21st
Location:	Virtual Meeting Room Link: https://202trail213.web.core.windows.net/ Also found on Doylestown Borough Website: https://www.doylestownborough.net/ And Doylestown Township Website: https://doylestownpa.org/
Contacts:	Steve DiSciullo - (215) 442-5329 - steven.disciullo@mbakerintl.com Stephanie Mason - (215) 348-9915 - sjmason@doylestownpa.org John Davis - (215) 345-4140 - jdavis@doylestownborough.net

The virtual public meeting room included:

- A video introduction by the Chair of the Bike and Hike Committee
- FAQ about trails
- Benefits of Trails
- General Information Sheet on Study Scope
- Local and Regional Context maps
- Alignment Alternative Maps
- Opportunities/Constraints list for each trail alternative
- Photo log of existing conditions in the study area
- Public Survey



Copies of this information is included in the Appendix.

The virtual public meeting room was promoted in a Intelligencer Newspaper article to further spread the word about the opportunity for the public to project input on the study.



Public On-Line Survey Results:

Over 143 people responded to the online public survey that was incorporated into the virtual public meeting room website. The survey results provided strong support for the trail project with over **85%** indicating they support this project. The survey questions and the full results of the survey are included in the Appendix.

2. Are you supportive of this trail concept?

💡 Insights

● Yes	122
● No	4
● Maybe (Explain in Question 4)	17



77% of the respondents indicated they would like a new connection to their neighborhood to the future trail.

5. Would you like us to explore an access point from your neighborhood to connect to this future trail?

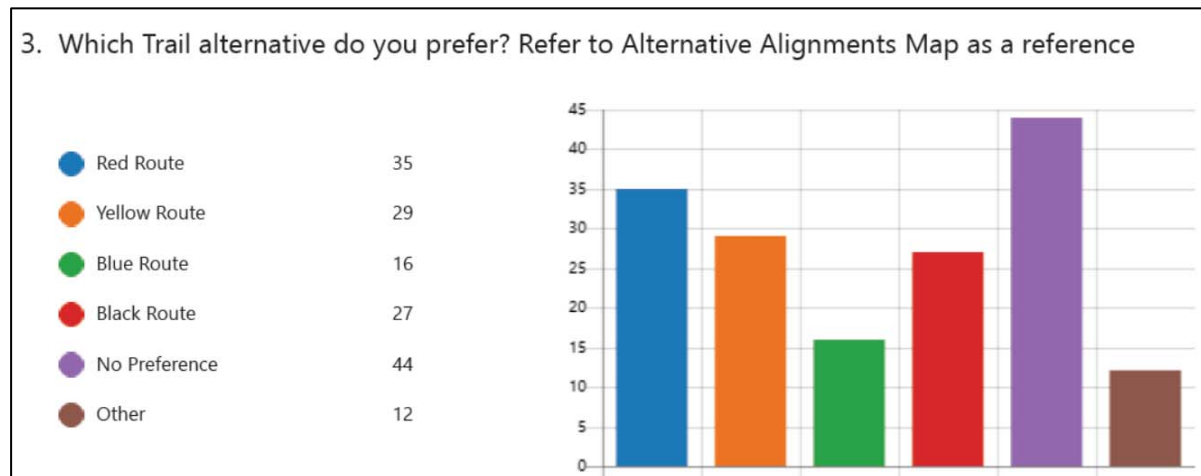
💡 Insights

- Yes - I would love a new conn... 90
- No - I prefer my neighborhoo... 27



The results of the question related to the preferred alignment presented a mixed result with the majority of the respondents indicating “no preference” for a particular alignment option:

- No Preference 27%
- Red Route 21%
- Yellow Route 18%
- Blue Route 10%
- Black Route 16%
- Other 8%

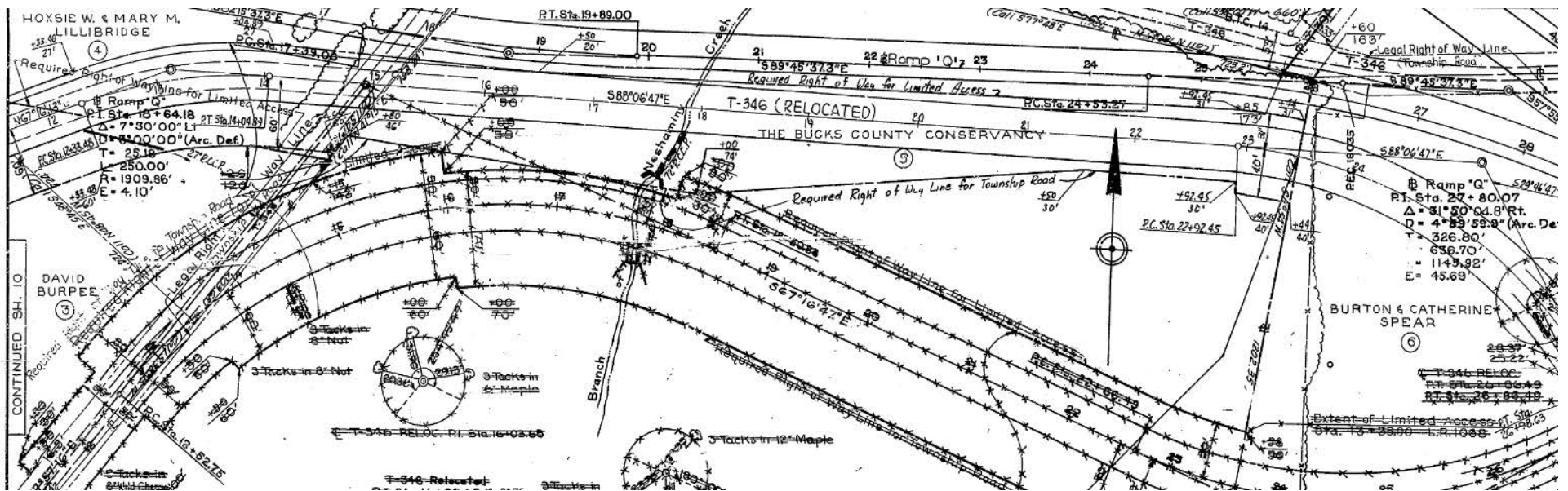


Many comments were received and were reviewed by the study team. They were incorporated into the alternative analysis and preferred alignment recommendation. Please see the Appendix for a full list of survey comments.

6) Right of Way Analysis

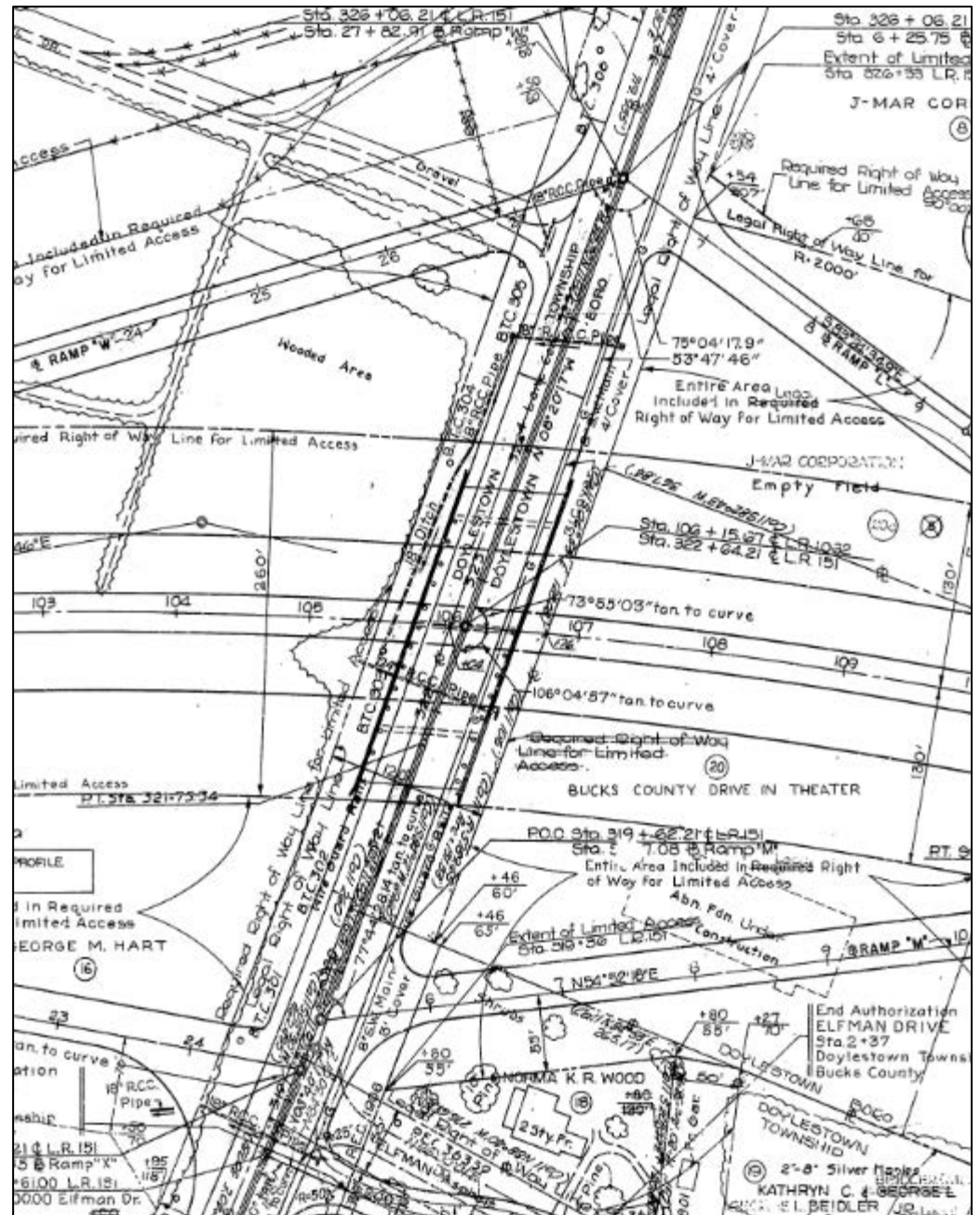
From a legal standpoint, the trail facility is planned to be located within public right of way, in publicly owned land, and/or in easements acquired by each of the municipalities where the trail is located. The following shows the legal right of way of the roadways involved in this study:

New Britain Road – This township road was relocated during the construction of the Route 202/611 bypass in the early 1970's as shown on the LR 1062 Section 1R/W below. The right of way varies from 60-80' since it is immediately adjacent to the limit access right of way for the bypass.



Easton Road Bridge and Ramps:

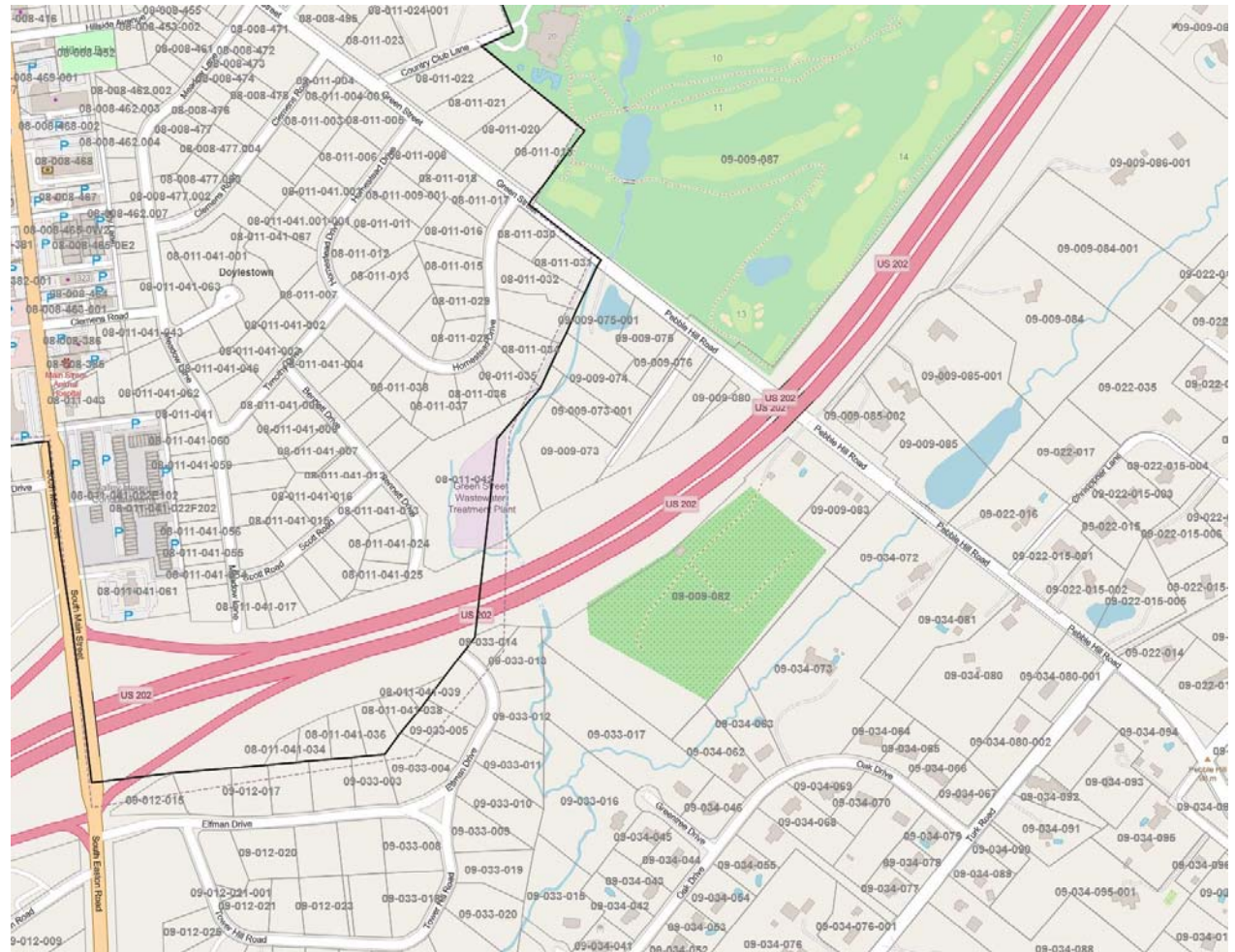
Route 202 passes under the Easton Road bridge and there is a diamond interchange between the two roadways. The original ROW plans show 40' to 70' of right of way outside the ramp areas that could be considered for the trail. It is anticipated that a structure mounted barrier will be needed if the trail needs to cross the Easton Road bridge.



Easton Road to Pebble Hill Road:

GIS data shows available right of way along Easton Road and the connecting ramps to the 202 Bypass. The limited access right of way continues on both sides of Route 202 bypass north from Easton Road to Pebble Hill Road. Route 202 is built in a fill condition in this area which occupies some of the right of way outside the roadway and shoulders. The Doylestown Country Club property is located to the west of Route 202 between Pebble Hill Road and East Road, a distance of approximately 3500’.

Near Pebble Hill Road, the side slopes of Route 202 are steeper as Route 202 rises to cross over Pebble Hill Road.



Pebble Hill Road to State Street/Route 313:

The GIS map for this area shows the large trailhead/park and ride lot and bike/hike trails installed as part of the PennDOT Pooles Corner project completed several years ago. There is ample right of way available around the stormwater basins and parking area.

The inset image shows a zoomed in view of the existing trails along East Road and East State Street near the signalized intersection with Route 202.



If is required to acquire the necessary right of way, easements and other agreements needed to construct long, linear projects like this one, the following well-established methods are available to obtain the land needed for the project:

- 1. Easements:** One of the least costly and most effective methods for acquiring the legal permission for the trail to cross a property is through a permanent easement agreement. This easement agreement would be a legal document that modifies the property owner's deed to allow the use of a portion of their property for the construction of the trail and permanent use of the area by trail users. A sample easement agreement successfully used by Doylestown Township on several previous trail projects is included in the Appendix. Typically, the municipality or other agency would be responsible for maintenance and liability issues related to the trail within the easement.
- 2. Lease Agreement:** This type of document is similar to an easement except that a specific time frame is stipulated in the agreement for use of the area for the trail. Some private property owners may have long range plans for their properties and may not want to have a permanent easement attached to their deed. However, these agreements are usually formed for relatively long time periods such as 10-20 years or more, are typically renewable and can function just as well as permanent easements.
- 3. Fee Simple Purchase:** Another more costly alternative and potentially time-consuming method is the outright purchase of portions of property from owners. If easements or other agreements cannot be formed, the County can negotiate a land purchase from a property owner for the trail. This process can often be complicated by disagreements over property appraisals, questions of property ownership, the need for right of way plans and modifications to deeds. However, this process is commonly used on transportation projects and could be used for trail projects as well.

PennDOT Right of Way Coordination:

Since the majority of this trail will be located in PennDOT Limited Access Right of Way, approval of the trail location and design will be required from PennDOT District 6-0. PennDOT does have provisions in its design manuals for the location of trails in limited access right of way and there are many precedents for this occurring, including the 202 Parkway Trail. On June 23, 2020, a conference call meeting was held with staff from PennDOT central office, PennDOT District 6-0, Doylestown Township, and Michael Baker Intl. The topic of discussion was focused on the SR 0202 limited access right-of-way and expectations/requirements of constructing a shared use path within limited access right-of-way. The following staff attended the meeting:

Roy Gothie – PennDOT statewide bicycle and pedestrian coordinator

Francis Haney – PennDOT D-6

Mary Ann Lang - PennDOT D-6

Chris Stanford – Project Manager, Michael Baker International

Stephanie Mason – Doylestown Township

David Tomko – Doylestown Township

Steve DiSciullo - Project Manager, Michael Baker International

Meeting summary: A shared use path within SR 0202 limited access right-of-way is possible with the adoption of an agreement between Doylestown Township and PennDOT. Since Doylestown Township has an existing Shared Use Path Maintenance agreement with PennDOT, a modification of the existing forms will be required. PennDOT will need to conduct a safety review of the proposed trail design prior to modifying the agreement. It is anticipated that the trail design will need to place the trail at a appropriate distance from the outside shoulder of the roadway, include appropriate guiderail/concrete barrier, include continuous right of way fencing between the roadway and trail as well as other improvements to assure safe operation of the roadway and trail. Official direction is progressing through Central Office for the use of Limited Access Right of way for trails and Roy mentioned that he has been working on the police for quite some time. The official policy is anticipated to be released in late 2021 or early 2022.

7) Implementation Plan

The proposed 202 to 313 trail outlined in this study is broken up into manageable construction projects based on starting points, ending points, connections, funding sources, and overall price. A map showing the sections follows below:



This map shows the 202 to 313 Trail broken up into four construction sections. Each has independent utility and connects important destinations in the project area.

A full-size version of this map is included in the Appendix.

The following list represents the locations and order in which the projects can feasibly be constructed.

Section #	Project Description	Cost	Recommended Funding Source(s)
1	Central Park to Warden Road Trail	\$762,360	DCED, DCNR
2	Warden Road Trail to Easton Road	\$2,110,135	TASA, DCED
3	Easton Road to Future Pebble Hill Road Trail	\$1,956,416	TASA, DCED
4	Future Pebble Hill Rd. to East State Street Trail (Poole's Corner)	\$1,538,941	TASA, DCED
	Total	\$6,367,851	

Refer to the Appendix for preliminary cost estimates of each project.

It is anticipated that the Doylestown Community Bike and Hike committee will take the lead for implementation of this potential trail. This committee has been in existence for over 25 years and has significant experience with the planning, design, and construction of trails. They will be assisted by the municipalities where the trails are located as required. This committee has the capacity to develop this trail. The committee is active and meets once a month to discuss ongoing efforts on numerous trail related projects. The committee is always pursuing grant and other funding opportunities for future projects. This committee, as well as the other municipalities involved in the study, has indicated their long-term commitment for trail development and maintenance.

Funding Options:

Securing adequate funding for the design and construction of these types of projects can be a challenge. The following is a list of possible funding sources for this project:

Pennsylvania Transportation Alternatives Program

The Transportation Alternatives Set-Aside (TASA) provides funding for projects and activities defined as transportation alternatives, including on- and off-road pedestrian and bicycle facilities, infrastructure projects for improving non-driver access to public transportation and enhanced mobility, community improvement activities, and environmental mitigation, trails that serve a transportation purpose, and safe routes to school projects.

<https://www.penndot.gov/ProjectAndPrograms/Planning/Pages/Transportation%20Alternatives%20Set-Aside%20-%20Surface%20Trans.%20Block%20Grant%20Program.aspx>



DCED Act 13 Grants: Greenways, Trails and Recreation Program (GTRP)

Act 13 of 2012 establishes the Marcellus Legacy Fund and allocates funds to the Commonwealth Financing Authority (the “Authority”) for planning, acquisition, development, rehabilitation and repair of greenways, recreational trails, open space, parks and beautification projects using the Greenways, Trails and Recreation Program (GTRP).

<http://www.newpa.com/find-and-apply-for-funding/funding-and-program-finder/greenways-trails-and-recreation-program-gtrp>

DCED Multimodal Transportation Fund

The Multimodal Transportation Fund provides grants to encourage economic development and ensure that a safe and reliable system of transportation is available to the residents of the commonwealth. Funds may be used for the development, rehabilitation and enhancement of transportation assets to existing communities, streetscape, lighting, sidewalk enhancement, pedestrian safety, connectivity of transportation assets and transit-oriented development.

<https://dced.pa.gov/programs/multimodal-transportation-fund/>

PennDOT Multimodal Transportation

Act 89 also established a dedicated Multimodal Transportation Fund that stabilizes funding for ports and rail freight, increases aviation investments, establishes dedicated funding for bicycle and pedestrian improvements, and allows targeted funding for priority investments in any mode.

<https://www.penndot.gov/ProjectAndPrograms/MultimodalProgram/Pages/default.aspx>

PENNDOT – Surface Transportation Program

The Twelve Year Transportation Program (as required by Act 120 of Pennsylvania State Law and its amendments) targets the Commonwealth's improvement efforts in all major transportation modes: highways, bridges, aviation, rail and transit. Transportation projects that focus on improving safety, enhancing mobility, moving goods and preserving the existing system are key to achieving the Department's goals and objectives.

The Division will continue to focus on incorporating the philosophy of the most current Federal and State Regulations in the continuous update of the Program; this includes the tie-in of planning requirements for Transportation Improvement Plans (TIPs), and the all encompassing State TIP (STIP). This program also involves the preparation of comprehensive information packages for key Department staff, the State Transportation Commission (STC), and elected state and federal legislators and officials. These packages facilitate and communicate the development of a transportation system responsive to the needs of the Commonwealth, monitors progress on key programs and projects, and aids in resolving outstanding



pennsylvania
DEPARTMENT OF COMMUNITY
& ECONOMIC DEVELOPMENT



Transportation Program issues. Staff and support services are also provided to the STC and other Program Center functions to prepare improvement programs which maintain and enhance the existing transportation system.

<http://www.dvrpc.org/TIP/>



Robert Wood Johnson Foundation

The mission of the Robert Wood Johnson Foundation is to improve the health and health care of all Americans. Our goal is clear: To help our society transform itself for the better.

Website: <http://www.rwjf.org/grants/>

William Penn Foundation

The William Penn Foundation, founded in 1945 by Otto and Phoebe Haas, is dedicated to improving the quality of life in eastern Pennsylvania through efforts that foster rich cultural expression, strengthen children's futures, and deepen connections to nature and community. In partnership with others, the Foundation works to advance a vital, just, and caring community.



<http://www.williampenntfoundation.org/Grants.aspx>



National Parks Service – Trails Assistance Program

The Rivers, Trails, and Conservation Assistance Program is the community assistance arm of the National Park Service. RTCA supports community-led natural resource conservation and outdoor recreation projects. RTCA staff provides technical assistance to communities so they can conserve rivers, preserve open space, and develop trails and greenways.

Website: <http://www.nps.gov/ncrc/programs/rtca/>

PA Department of Conservation and Natural Resources – Keystone Grant Program and Recreational Trails Program

Established on July 1, 1995, the Pennsylvania Department of Conservation and Natural Resources is charged with maintaining and preserving the 117 state parks; managing the 2.1 million acres of state forest land; providing information on the state's ecological and geologic resources; and establishing community conservation partnerships with grants and technical assistance to benefit rivers, trails, greenways, local parks and recreation, regional heritage parks, open space and natural areas.



Local governments, county governments and non-profit organizations can apply for Community Conservation Partnerships Program (C2P2) funding to assist them with addressing their recreation and conservation needs as well as supporting economically beneficial recreational tourism initiatives.

[Website: https://www.dcnr.pa.gov/Communities/Grants/TrailGrants/Pages/default.aspx](https://www.dcnr.pa.gov/Communities/Grants/TrailGrants/Pages/default.aspx)

Contact:

Northeast Regional Office: (Region 2)

Jean Lynch | Regional Advisor: 610-892-3903

email: jealynch@pa.gov

The following local funding sources may also be available:

- County, City, Borough and Township funds
- Private sponsorships, local fund raisers, etc.
- County Open Space funds

Next Steps

The study identified several alternative alignments, evaluated the feasibility of these alternatives, and recommended a preferred trail alignment. The next steps to bring this study's findings into active use includes:

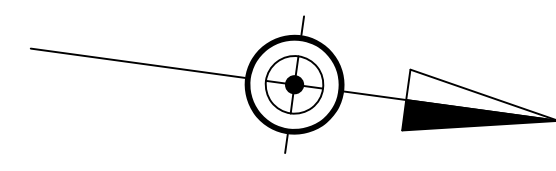
- Explore Grant Opportunities for Engineering Design and Construction Funding
- Complete Engineering Design
- Complete Project Construction

Regular coordination with key stakeholders will be necessary in the development of the network of trails identified in this study. This includes coordination with PennDOT District 6-0, Doylestown Township, and Doylestown Borough. As trail design moves forward, it is critical to continue to engage the local community to receive feedback and communicate progress.

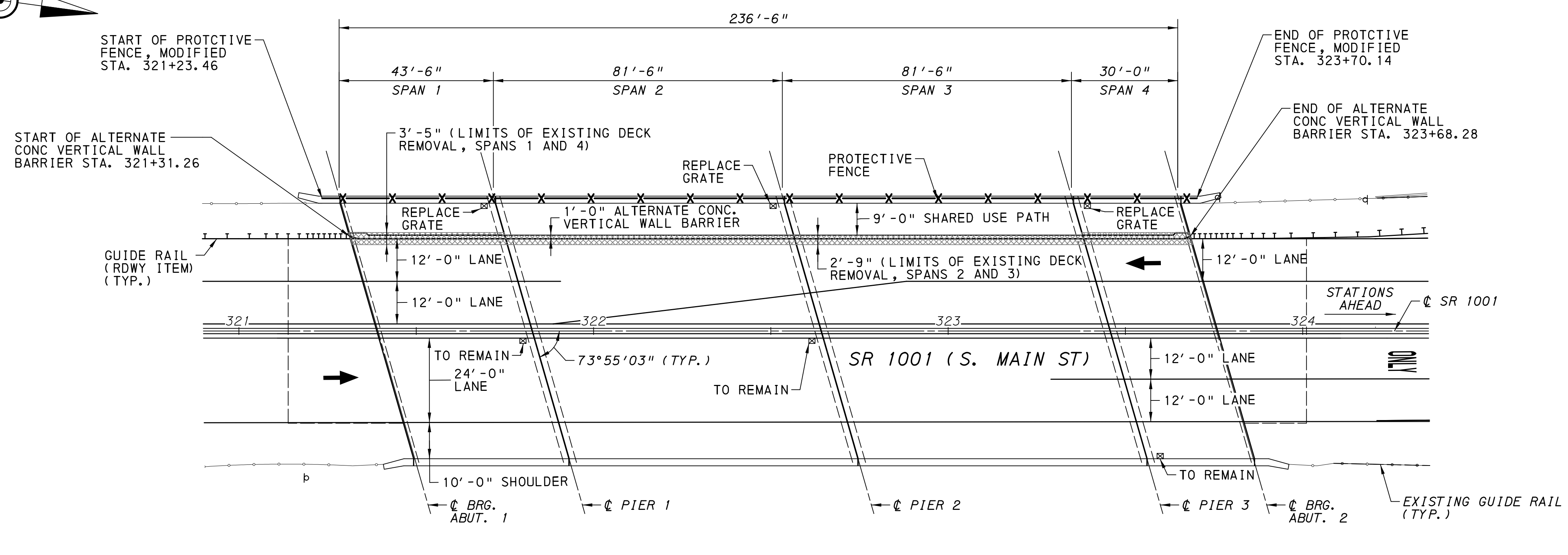
In collaboration with the steering committee members, the funding sources will be explored with the ultimate goal of completing this important connection project for Doylestown Township, and Borough Residents.

Appendix

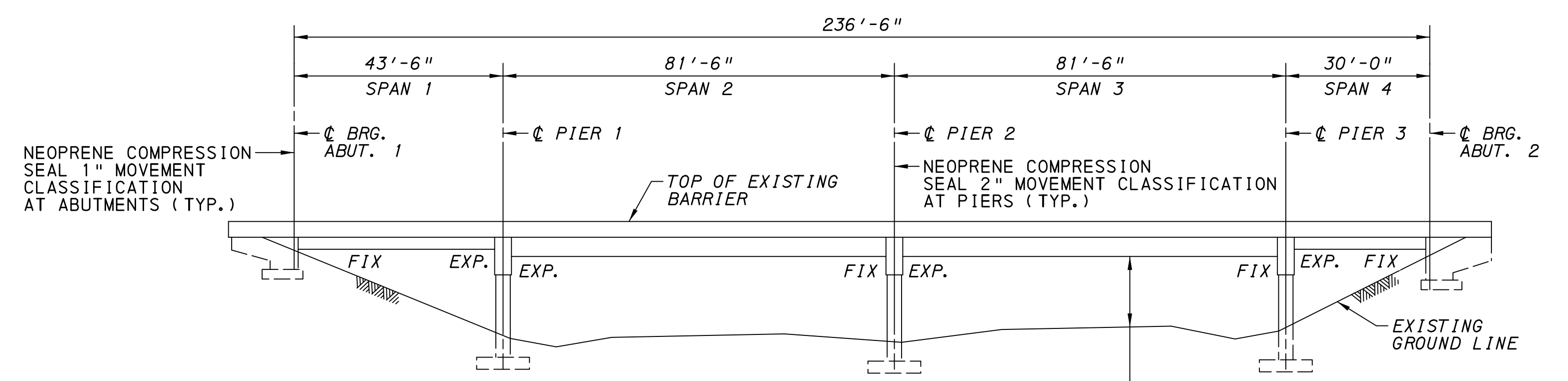
Conceptual Structure Mounted Barrier Plans for Easton Road over SR 202



INDEX OF DRAWINGS	
SHEET	TITLE
1	GENERAL PLAN & ELEVATION
2	GENERAL NOTES
3	LOAD RATINGS & CROSS SECTION
4	QUANTITIES & MISC. DETAILS
5	STAGED CONSTRUCTION SECTIONS
6	REMOVAL PLAN
7	BARRIER PLAN - 1
8	BARRIER PLAN - 2
9	JOINT DETAILS
10	REBAR SCHEDULE



PLAN
0 20 40 FEET



ELEVATION
0 20 40 FEET

LEGEND

- ➔ - TRAFFIC DIRECTION
- ⊗ - EXISTING TYPE B SCUPPER
- ⊕ - EXISTING SIGN
- X— - PROTECTIVE FENCE
- ▨ - REMOVAL OF EXISTING DECK

Mark	Description	By	Chk'd	Recm'd	Date
REVISIONS					

** REFERENCED FROM EXISTING DRAWINGS SEE S-22240 PLANS.

PROTECTIVE FENCE	BC-701M	JAN. 31, 2019
THREE-BEAM TO VERTICAL WALL BRIDGE BARRIER TRANSITION CONNECTION	BC-703M	AUG. 4, 2017
ALTERNATE RAILING DETAILS	BC-718M	SEPT 30, 2016
TEMPORARY CONCRETE BARRIER, STRUCTURE MOUNTED	BC-719M	JAN. 31, 2019
REINFORCEMENT BAR FABRICATION DETAILS	BC-736M	JAN. 31, 2019
BRIDGE DRAINAGE	BC-751M	JAN. 31, 2019
CONCRETE DECK SLAB DETAILS	BC-752M	SEPT 30, 2016
PREFORMED NEOPRENE COMPRESSION SEAL JOINT FOR APPROACH SLABS	BC-766M	JAN. 31, 2019
TYPICAL WATERPROOFING AND EXPANSION DETAILS	BC-788M	JAN. 31, 2019
UTILITY ATTACHEMENT AND SUPPORT DETAILS, PRESTRESSED BRIDGES	BC-794M	JAN. 31, 2019
DESCRIPTION	DWG. NO.	APP. DATE

SUPPLEMENTAL DRAWINGS

PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA

Matthew J. Hickson
REG. PROF. ENGINEER
DATE 12-3-2019

**COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION**

**BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
GENERAL PLAN & ELEVATION**

RECOMMENDED _____

DISTRICT BRIDGE ENGINEER

SHEET 1 OF 10
& SUPPLEMENTAL DRAWINGS

S -

APPROXIMATE QUANTITIES - BRIDGE STRUCTURE, AS DESIGNED

ITEM NO.	ITEM	UNIT	SUPERSTRUCTURE	TOTAL
1001-0001	CLASS AA CEMENT CONCRETE (5) (3)	CY	24	24
1001-0960	CLASS AAAP CEMENT CONCRETE (2)	CY	21	21
1002-0052	REINFORCEMENT BARS, EPOXY COATED	LB	5145	5145
1018-0050	REMOVAL OF PORTION OF EXISTING BRIDGE (4)	LS	1	1
1019-0050	PROTECTIVE COATING FOR REINFORCED CONCRETE SURFACES (PENETRATING SEALERS, BRIDGE SUPERSTRUCTURE)	SY	169	169
1016-0006	BARRIER PROTECTIVE FENCE, VINYL COATED STEEL (6)	LF	247	247
1022-0001	STEEL BRIDGE HAND RAILING	LF	237	237
1008-0002	PREFORMED NEOPRENE COMPRESSION JOINT SEAL, 1" MOVEMENT (LF)	LF	81	81
1008-0004	PREFORMED NEOPRENE COMPRESSION JOINT SEAL, 2" MOVEMENT (LF)	LF	122	122
9000-0096	ALTERNATE STRUCTURAL STEEL SCUPPER GRATE, (BICYCLE SAFE GRATE) (1)	EACH	3	3

(1) SEE SPECIAL PROVISIONS

(2) INCLUDES CLASS AAAP CONCRETE IN DECK SLAB.

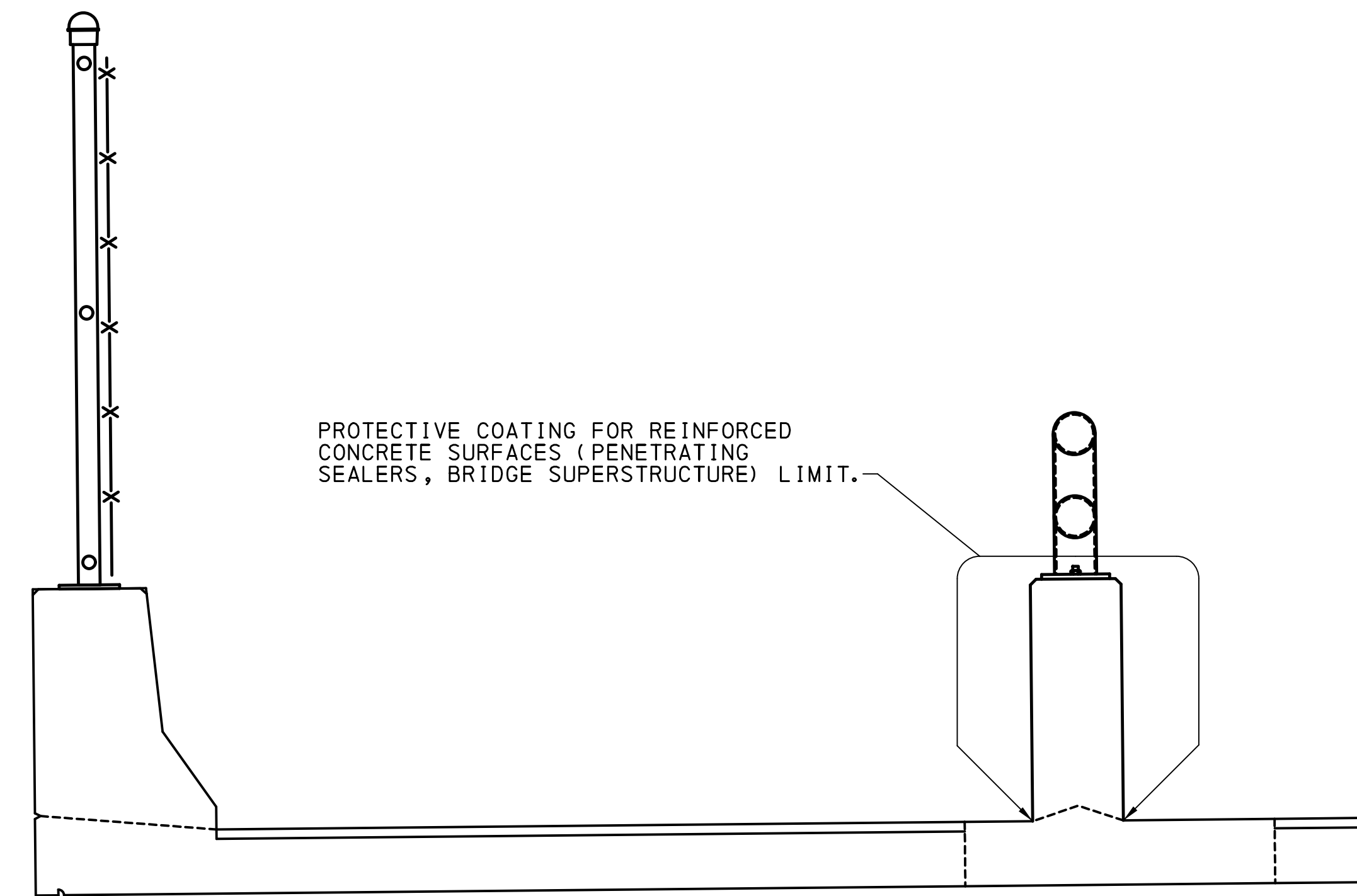
(3) INCLUDES CLASS AA CONCRETE IN BARRIERS.

(4) INCLUDES REMOVAL OF DECK FOR ALTERNATE HEIGHT CONCRETE VERTICAL WALL BARRIER PLACEMENT.

REMOVE SIP FORMS AT NO ADDITIONAL COST TO THE BOROUGH IF SIP FORMS DISLODGE DURING DECK REMOVAL. PROVIDE FORMWORK FOR DECK PLACEMENT IN THE EVENT THE SIP FORMS ARE DISLODGED AT NO ADDITIONAL COST TO THE BOROUGH.

(5) METAL CURB DRAINS ARE INCIDENTAL TO THE BARRIER AND BARRIER CONCRETE.

(6) DOWEL HOLES AND ADHESIVE ANCHORS FOR THE INSTALLATION OF THE POSTS ARE INCIDENTAL TO ITEM 1016-0006.



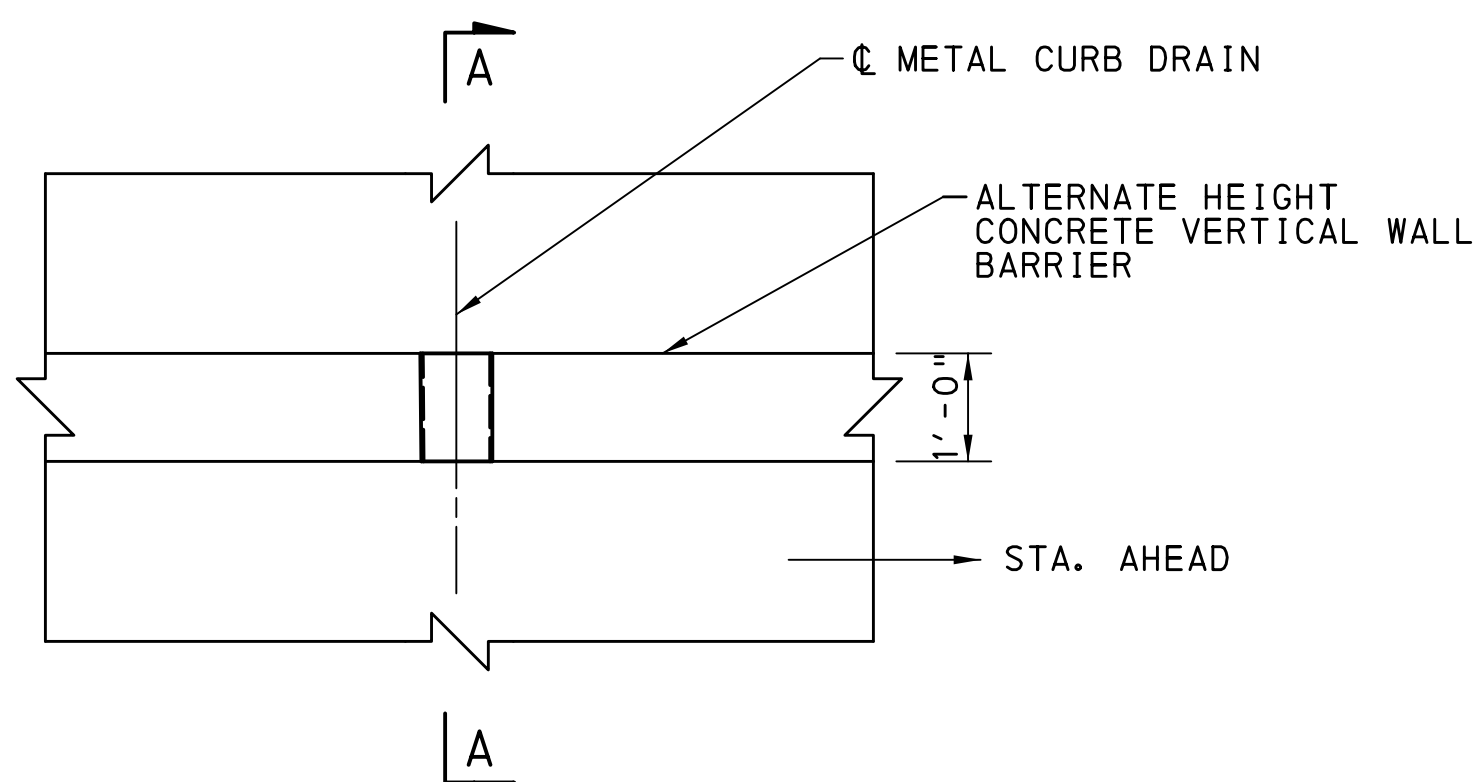
PROTECTIVE COATING FOR REINFORCED CONCRETE SURFACES (PENETRATING SEALERS, BRIDGE SUPERSTRUCTURE) LIMIT.

PROTECTIVE COATING LIMITS

(ITEM 1019-0050)

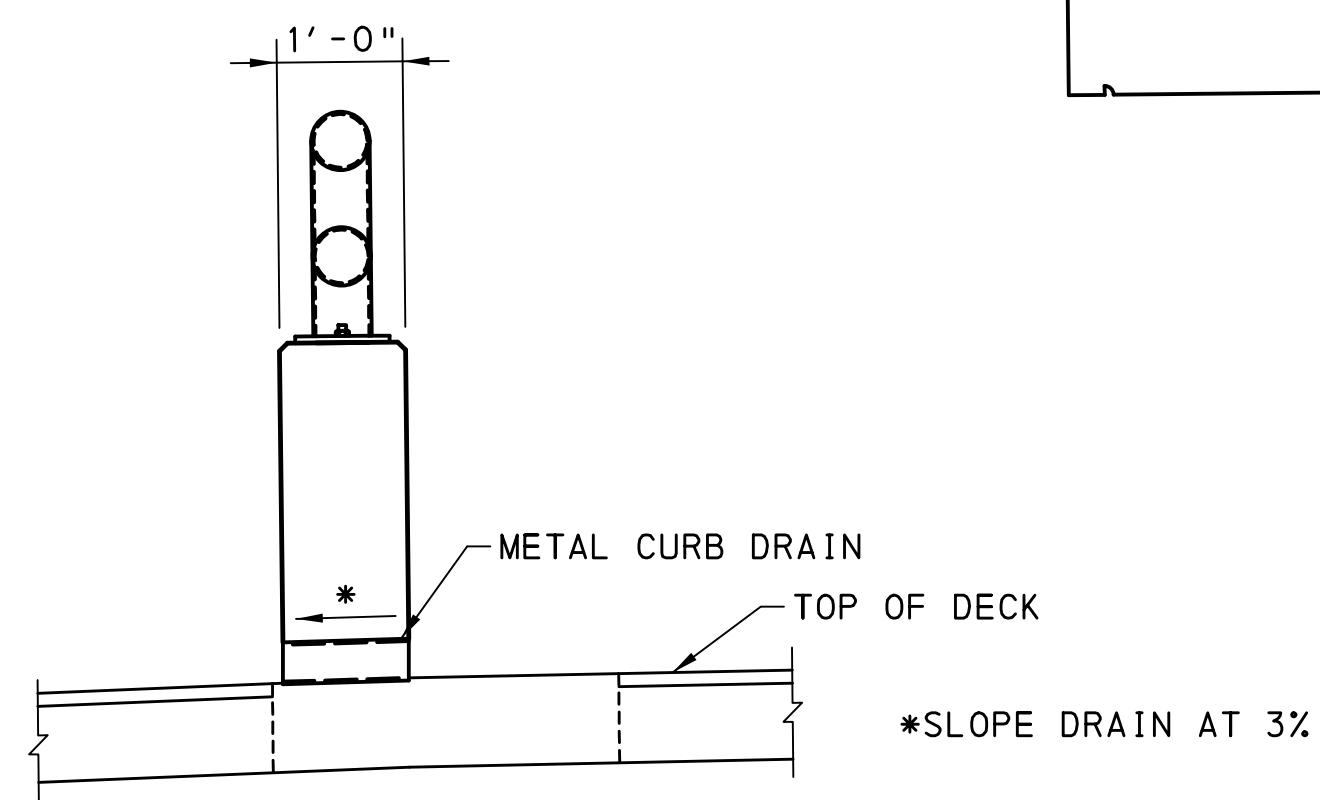
NOT TO SCALE

NOTE:
ALTERNATE RAILING NOT SHOWN FOR CLARITY



PLAN

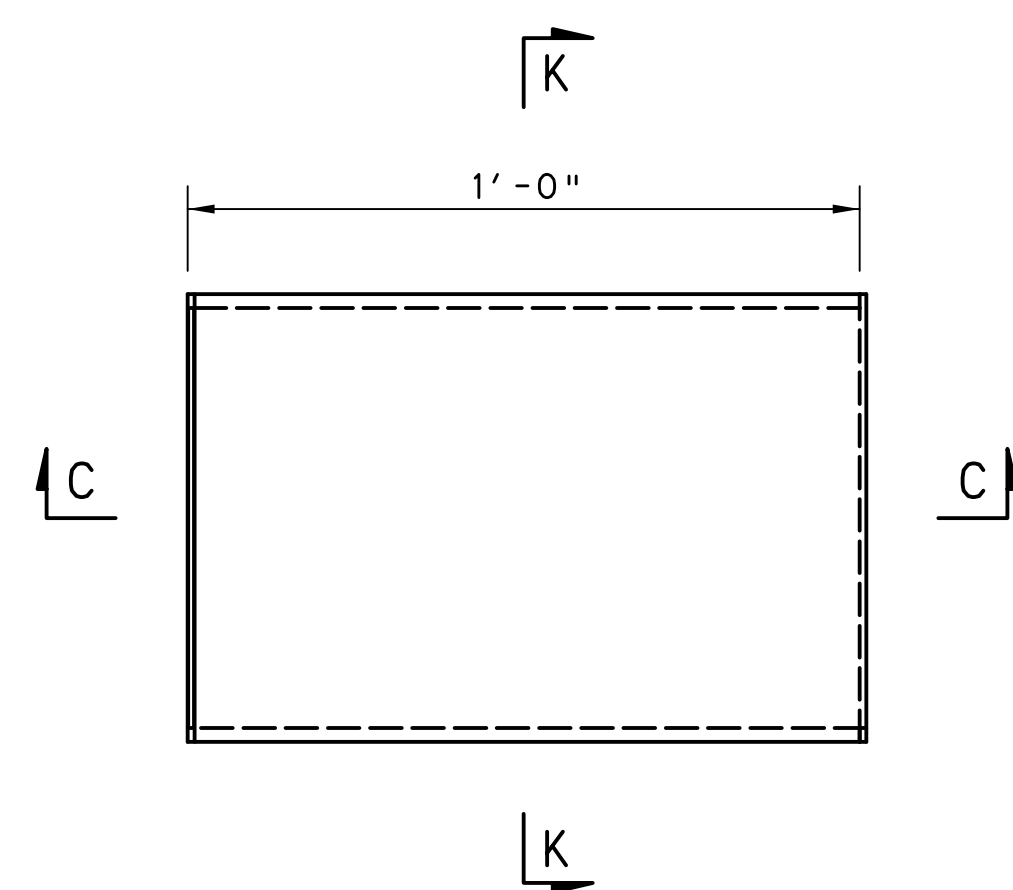
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SECTION A-A

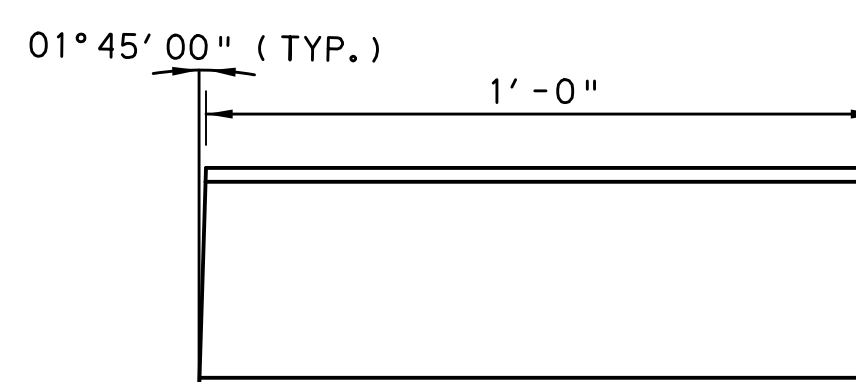
NOT TO SCALE

DECK DRAIN DETAILS



METAL CURB DRAIN PLAN

NOT TO SCALE



SECTION C-C

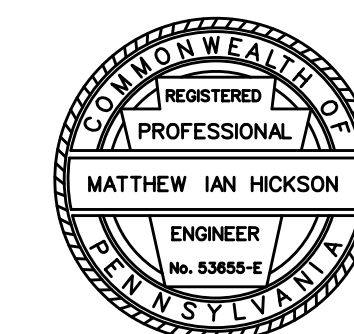
NOT TO SCALE

NOTES

- FOR GENERAL NOTES SEE SHEET 2.
- FOR DRAIN LOCATIONS SEE SHEETS 7 AND 8.
- FOR ADDITIONAL DECK DRAIN DETAILS AND INFORMATION SEE STANDARD DRAWING BC-751M.
- FOR SECTION K-K, SEE STANDARD DRAWING BC-751M SHEET 5 OF 7.

Mark	Description	By	Chk' d.	Recm' d.	Date
REVISIONS					

PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



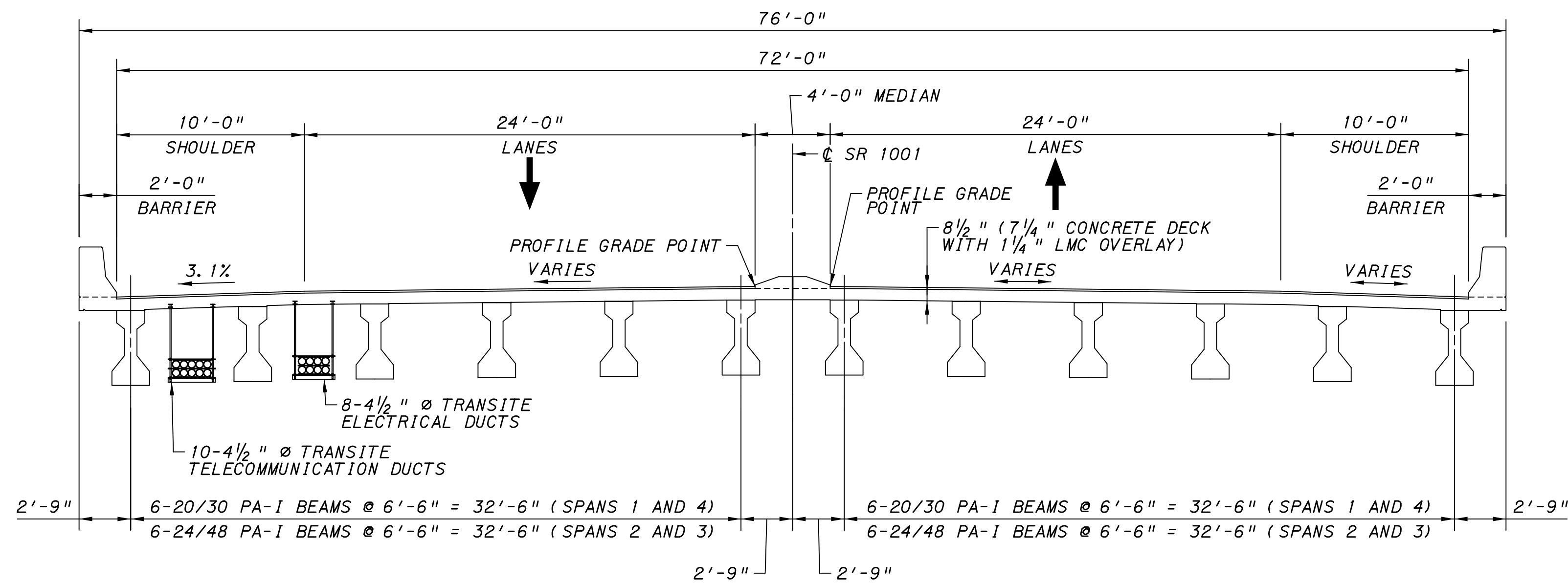
**COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION**

**BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
QUANTITIES & MISC. DETAILS**

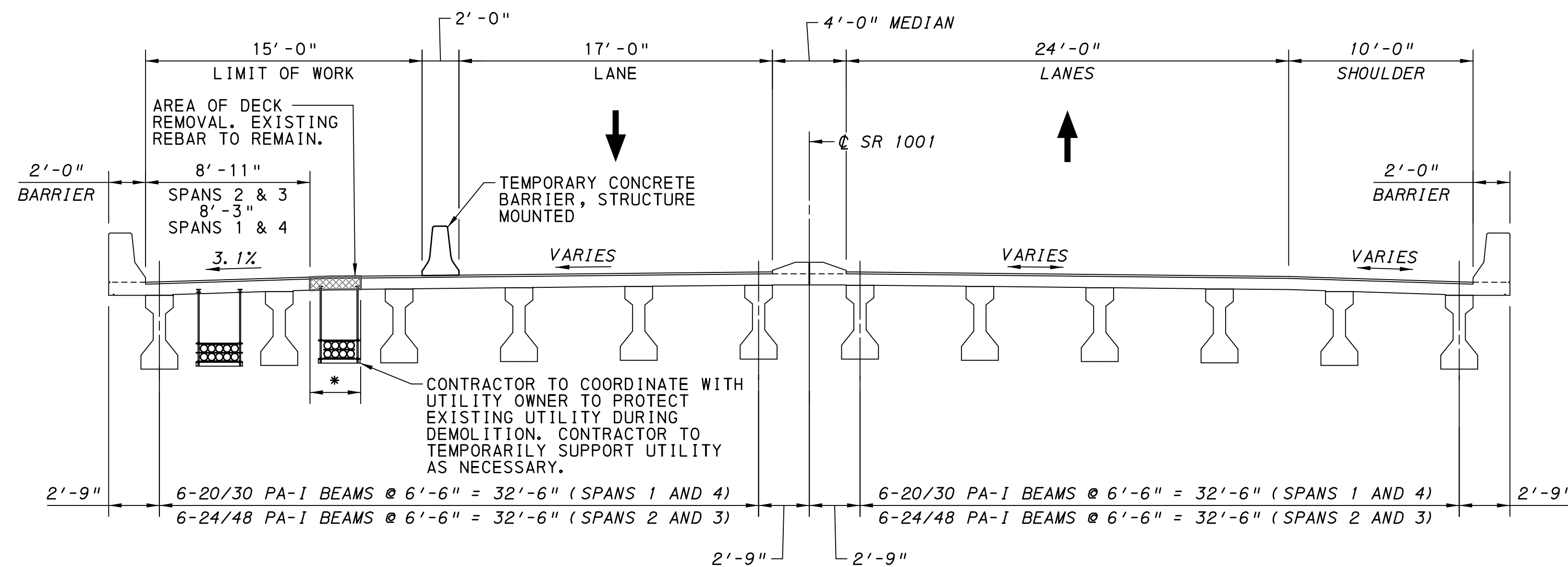
RECOMMENDED _____

SHEET 4 OF 10

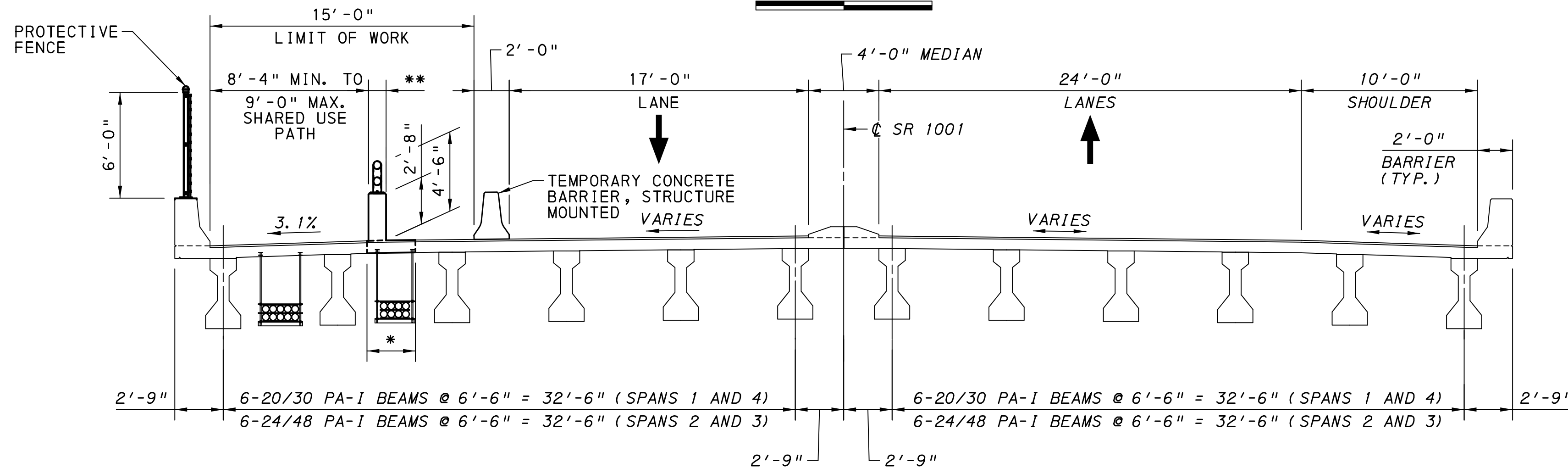
S-



EXISTING TYPICAL SECTION



STAGE 1 TYPICAL SECTION



STAGE 2 TYPICAL SECTION



NOTE: MATCH EXISTING CROSS SLOPES

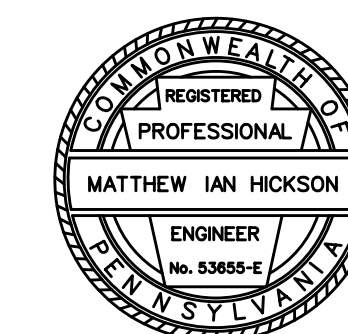
NOTES

- FOR GENERAL NOTES, SEE SHEET 2.
- FOR PROPOSED TYPICAL SECTION, SEE SHEET 3.

Mark	Description	By	Chk'd.	Rec'd.	Date
REVISIONS					

- * 2'-9" SPANS 2 AND 3.
3'-5" SPANS 1 AND 4.
- ** ALTERNATE CONCRETE VERTICAL WALL BARRIER WITH ALTERNATE RAILING

PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



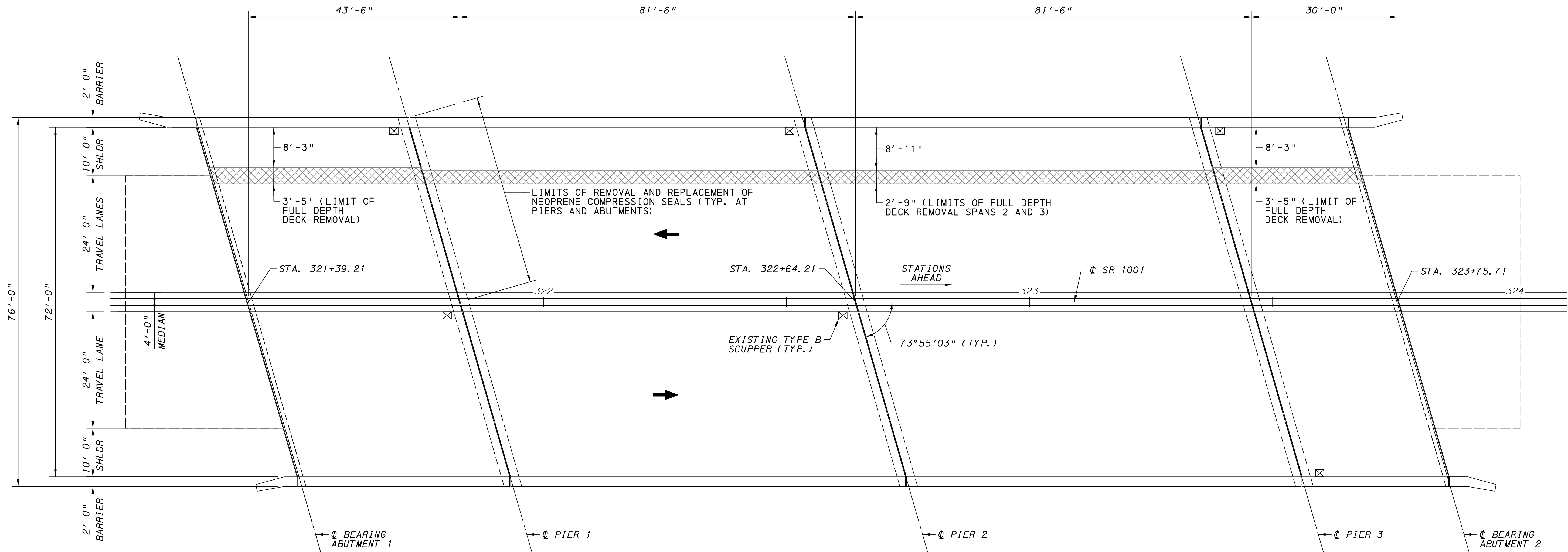
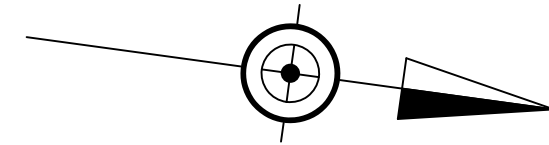
COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
STAGED CONSTRUCTION SECTIONS

RECOMMENDED _____

SHEET 5 OF 10

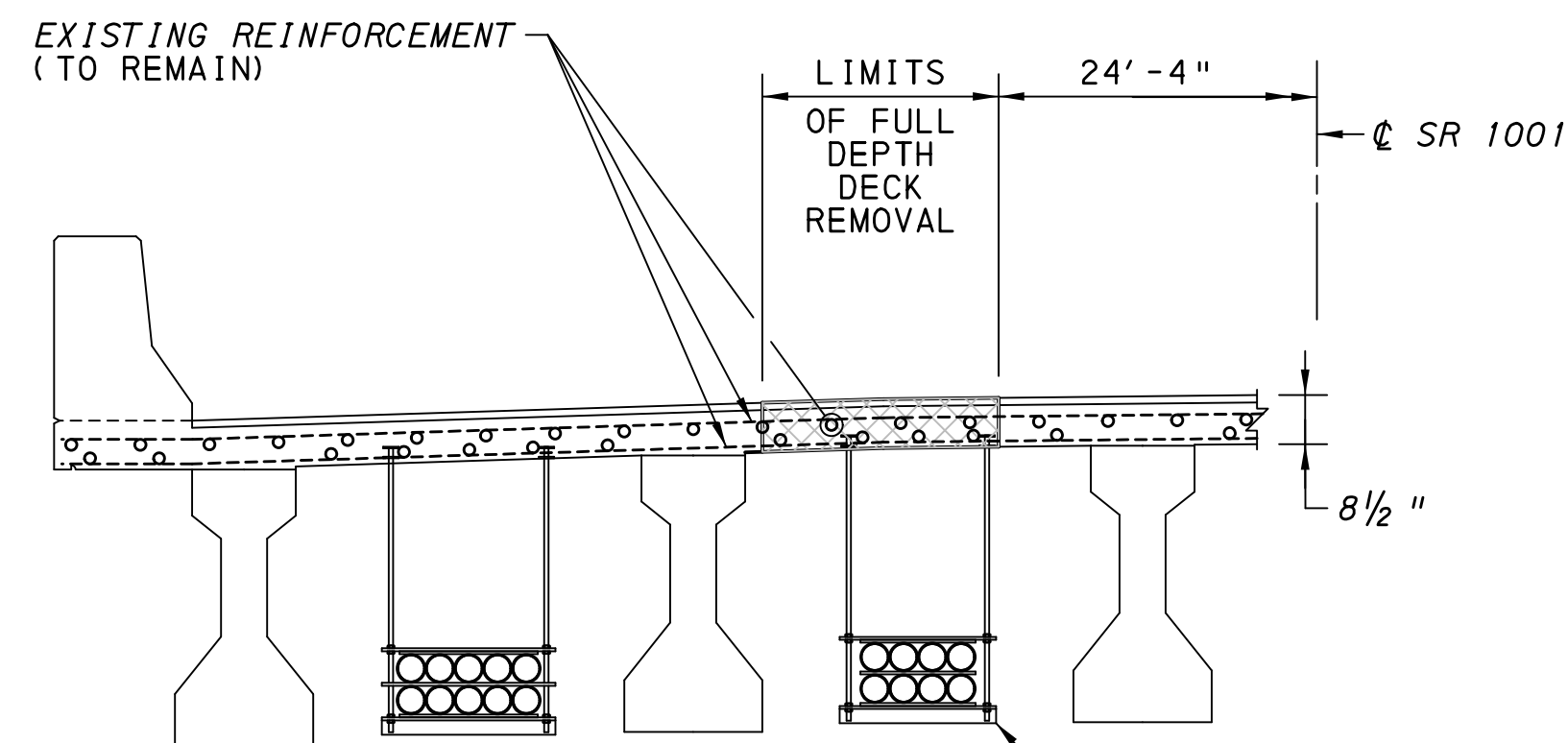
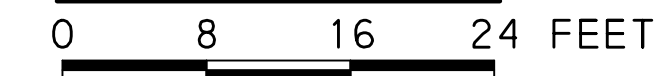
S-



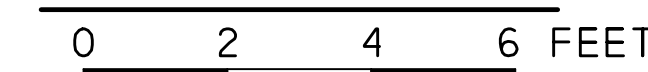
LEGEND

- TRAFFIC DIRECTION
- REMOVAL OF EXISTING DECK

REMOVAL PLAN



REMOVAL DETAIL



LOOKING AHEAD STATIONS

CONTRACTOR RESPONSIBLE TO COORDINATE WITH UTILITIES, PROTECT THEM FROM DAMAGE AND TEMPORARILY SUPPORT THEM IF NECESSARY. ANY TEMPORARY SUPPORT SYSTEM USED CANNOT REDUCE THE VERTICAL CLEARANCE BELOW SPANS 2 AND 3 BELOW THE EXISTING CLEARANCE OR 16'-6", WHICH EVER IS LESS.

NOTES

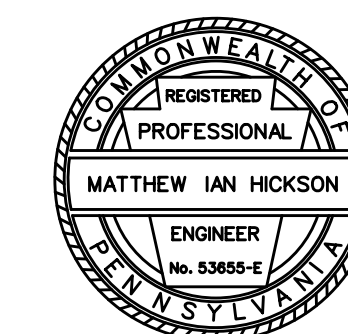
- FOR GENERAL NOTES, SEE SHEET 2.
- FOR BARRIER PLANS, SEE SHEETS 7 AND 8.
- FOR JOINT DETAILS, SEE SHEET 9.
- REMOVAL OF THE EXISTING DECK WITHIN THE LIMITS INDICATED ON THIS SHEET ARE INCIDENTAL TO THE REMOVAL OF PORTION OF EXISTING BRIDGE ITEM.

Mark	Description	By	Chk'd.	Recm'd.	Date
REVISIONS					

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
REMOVAL PLAN

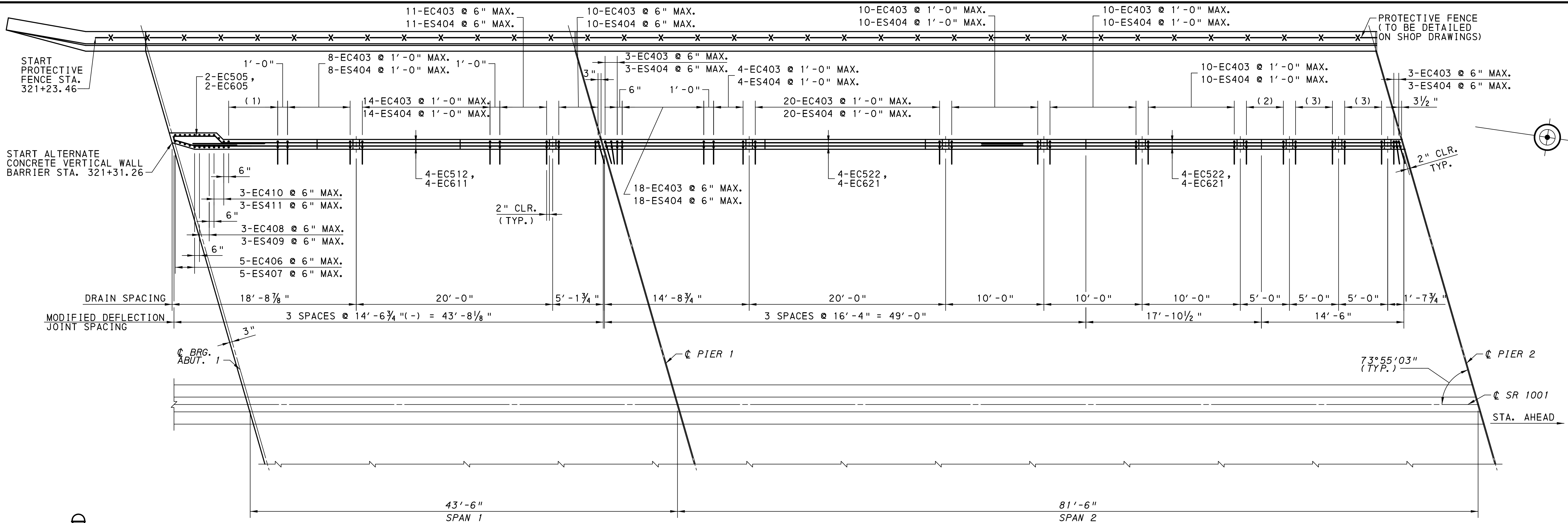
PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



RECOMMENDED _____

SHEET 6 OF 10

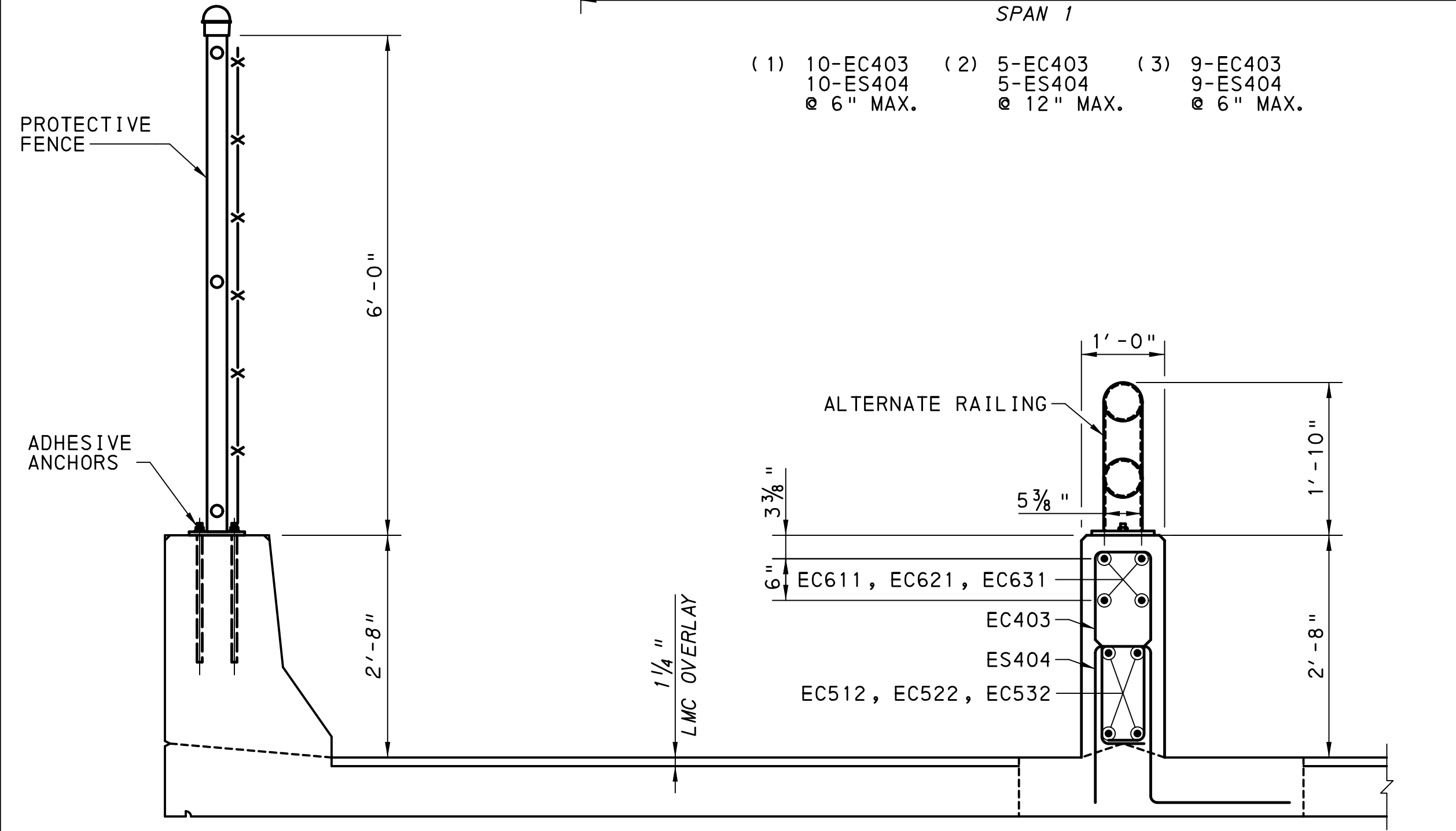
S-



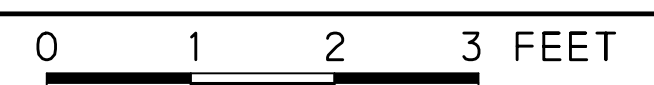
BARRIER AND RAILING PLAN - SPANS 1 & 2



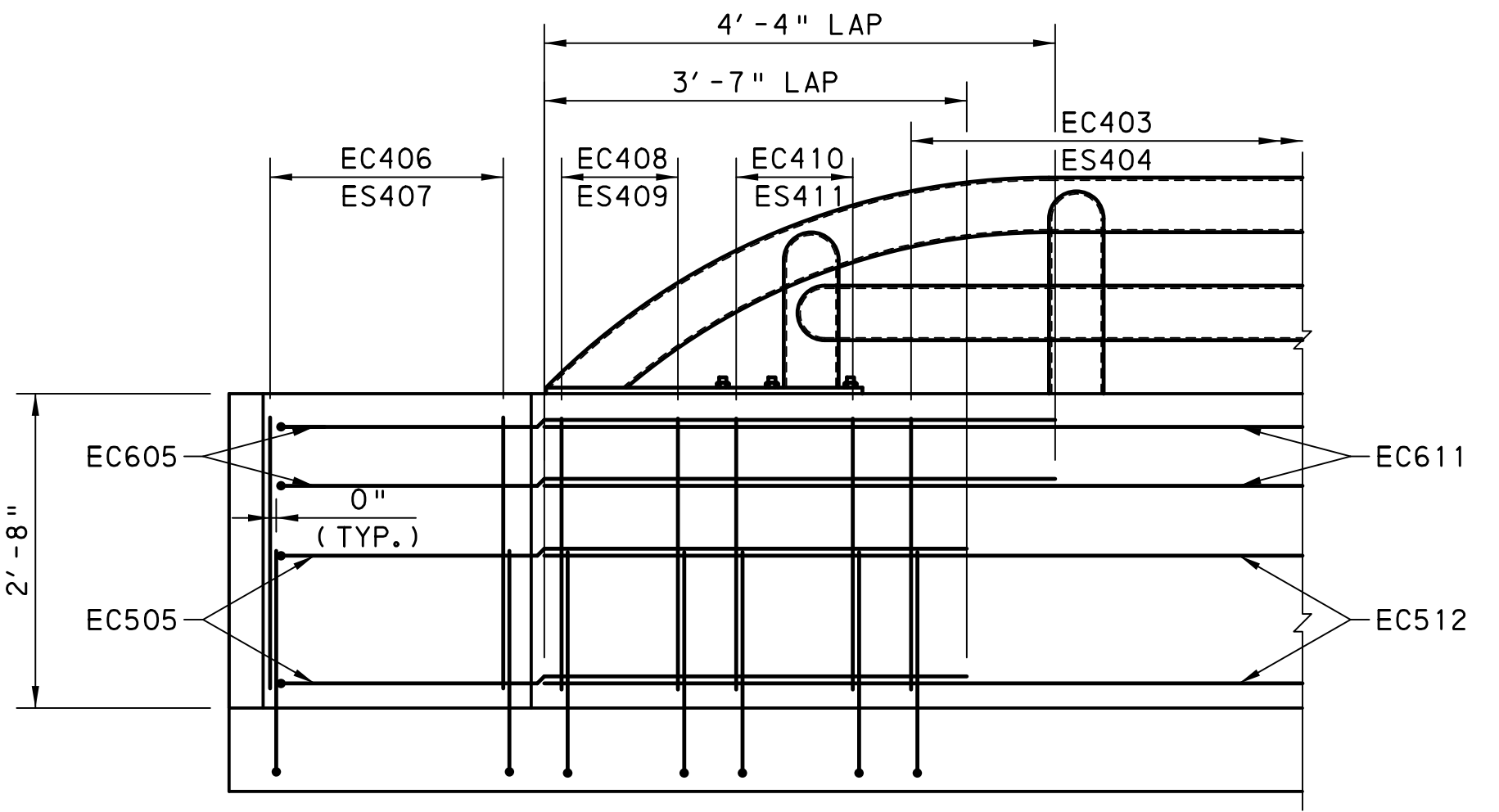
- (1) 10-EC403 @ 6" MAX.
- (1) 10-ES404 @ 6" MAX.
- (2) 5-EC403 @ 12" MAX.
- (2) 5-ES404 @ 12" MAX.
- (3) 9-EC403 @ 6" MAX.
- (3) 9-ES404 @ 6" MAX.



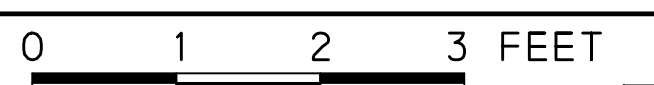
PROPOSED ALT. CONCRETE VERTICAL WALL BARRIER AND FENCE DETAIL



NOTE: BEAMS NOT SHOWN FOR CLARITY



TERMINATION SECTION ELEVATION VIEW - NEAR



NOTES

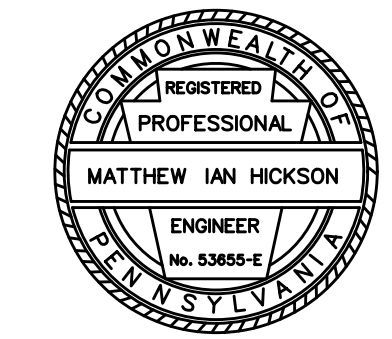
- FOR GENERAL NOTES, SEE SHEET 2.
- FOR REINFORCEMENT BAR SCHEDULE, SEE SHEET 10.
- WORK THIS SHEET WITH SHEET 8.
- MINIMUM BARRIER REINFORCEMENT BAR LAP LENGTHS, UNLESS OTHERWISE NOTED ARE 3'-4" FOR #5 BARS AND 3'-11" FOR #6 BARS.

Mark	Description	By	Chk'd	Rec'd	Date
REVISIONS					

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
BARRIER PLAN - 1

PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA

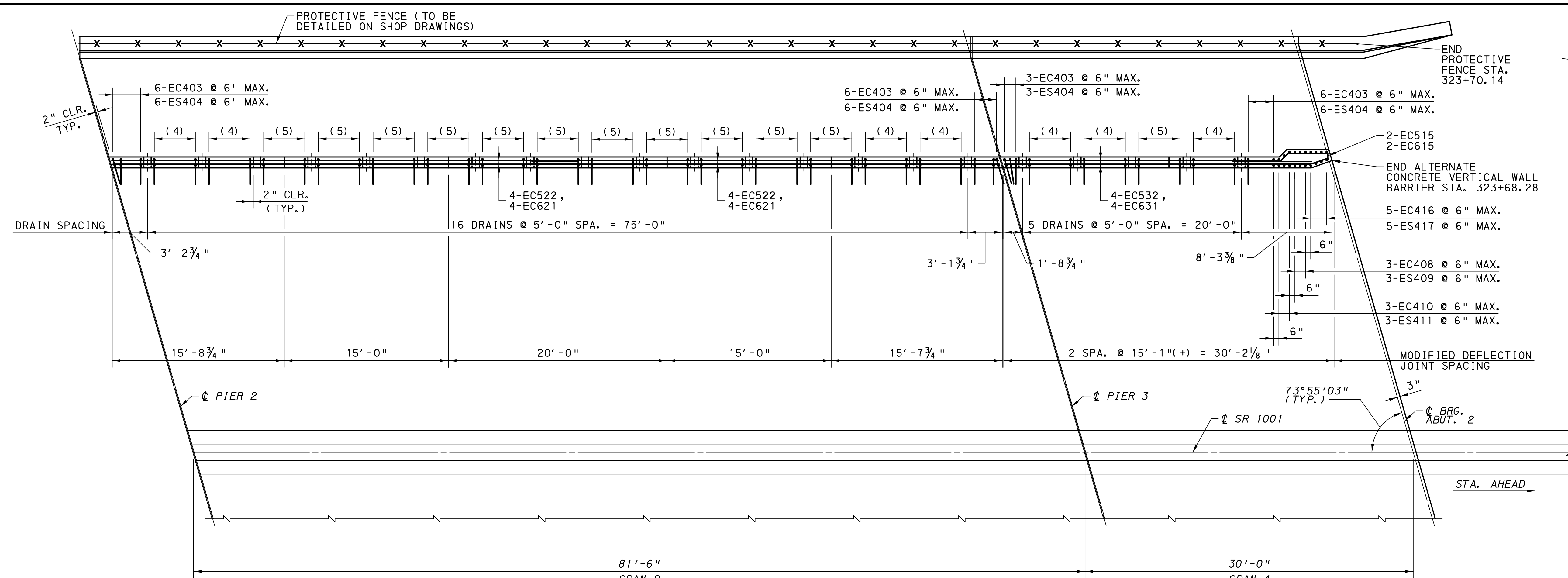


RECOMMENDED _____

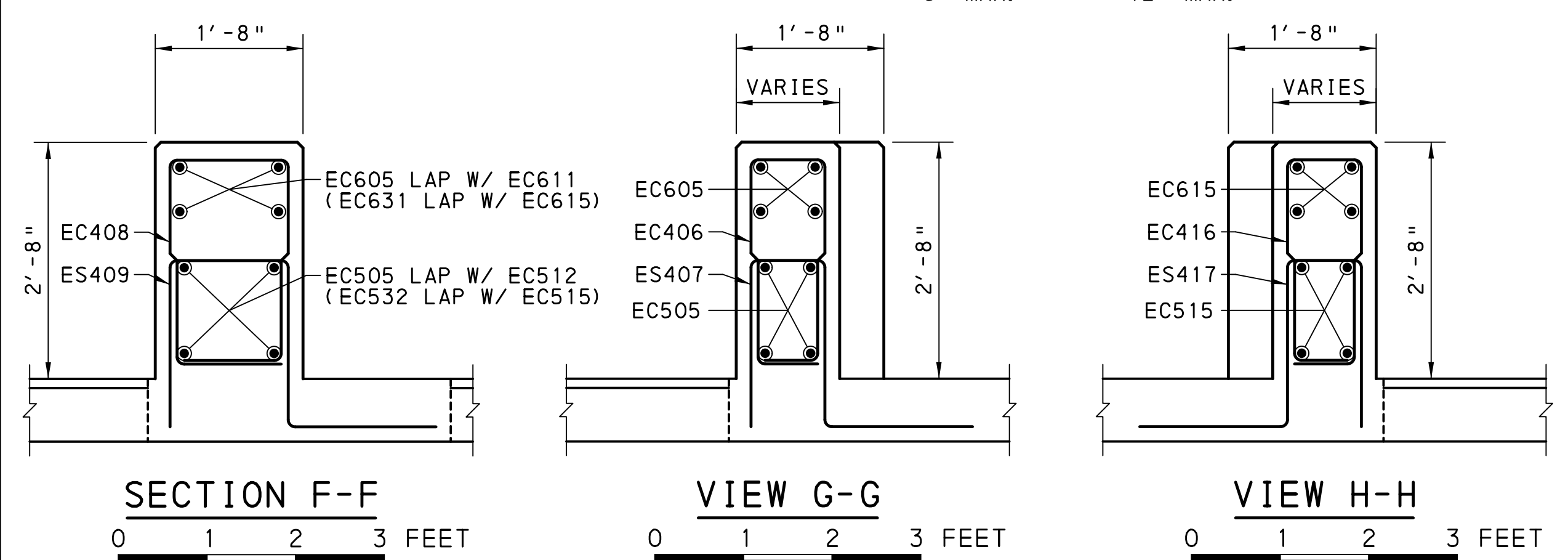
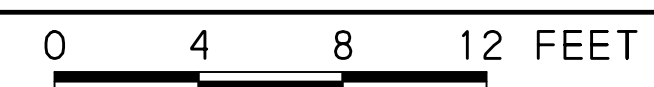
SHEET 7 OF 10

S-

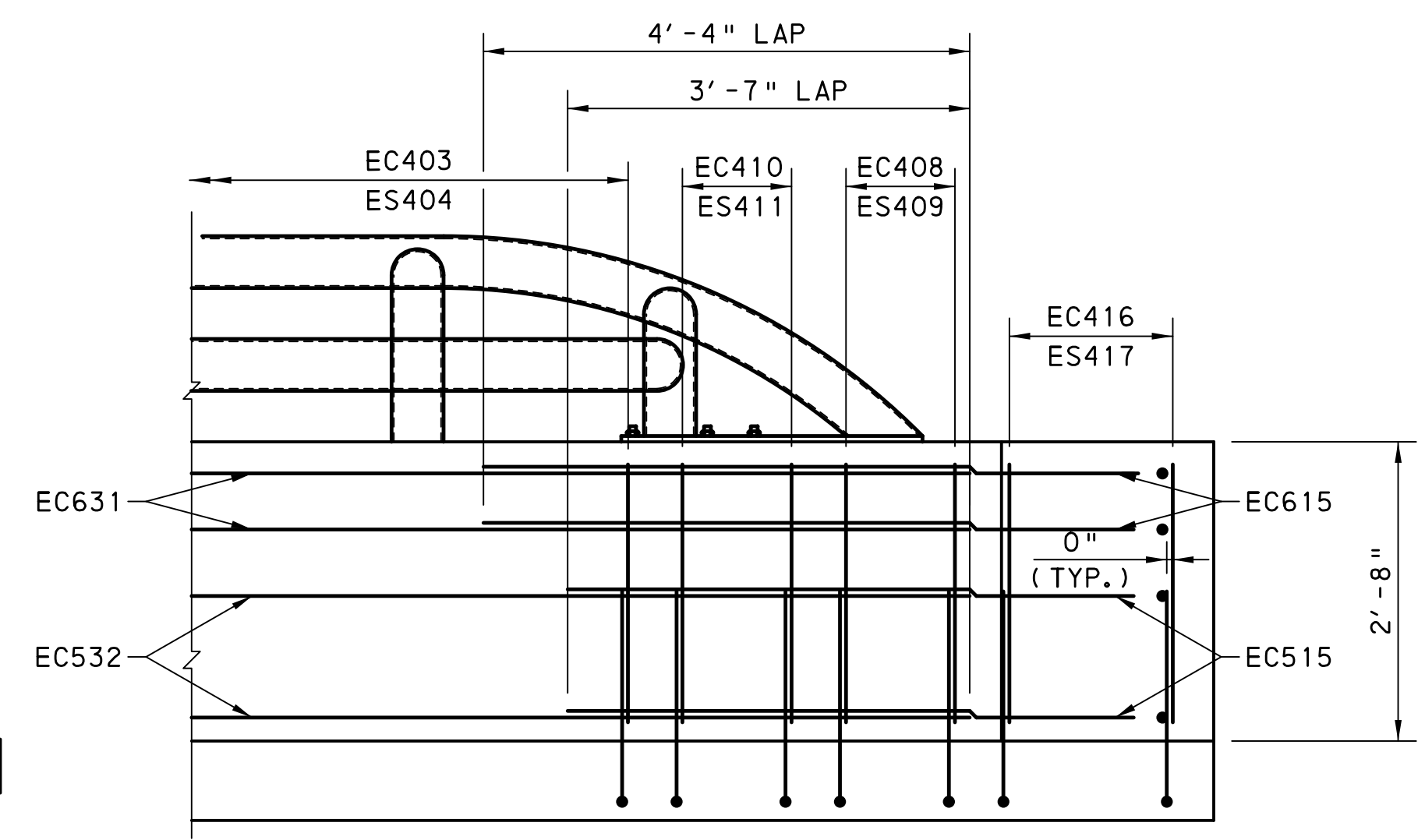
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 8/23/2019 8:03:21 AM 115-3333 f.schmid



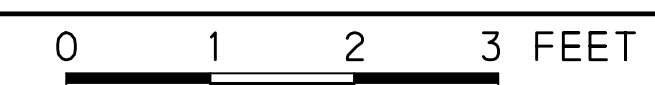
BARRIER AND RAILING PLAN - SPANS 3 & 4



NOTE: RAILING NOT SHOWN FOR CLARITY BARS MARKED WITH () AT THE FAR END OF THE BRIDGE



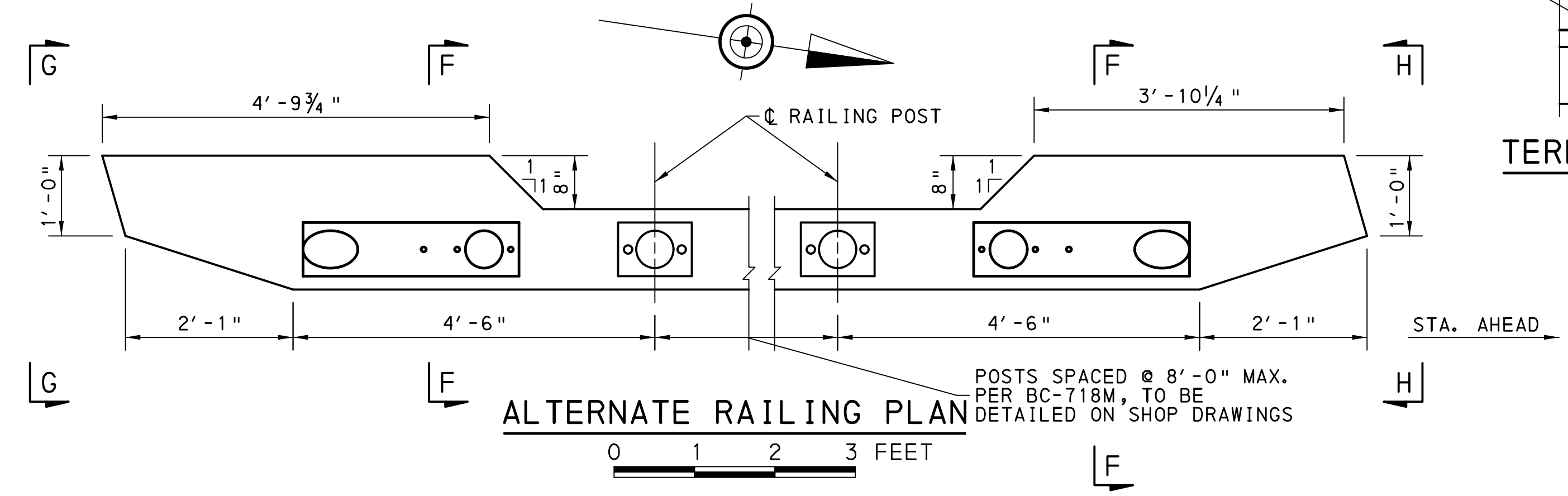
TERMINATION SECTION ELEVATION VIEW - FAR



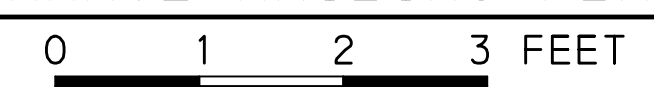
NOTES

- FOR GENERAL NOTES, SEE SHEET 2.
- FOR REINFORCEMENT BAR SCHEDULE, SEE SHEET 10.
- WORK THIS SHEET WITH SHEET 7.
- MINIMUM BARRIER REINFORCEMENT BAR LAP LENGTHS, UNLESS OTHERWISE NOTED ARE 3'-4" FOR #5 BARS AND 3'-11" FOR #6 BARS.

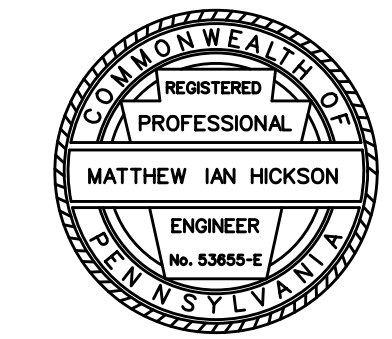
Mark	Description	By	Chk'd	Recm'd	Date
REVISIONS					



ALTERNATE RAILING PLAN



PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
BARRIER PLAN - 2

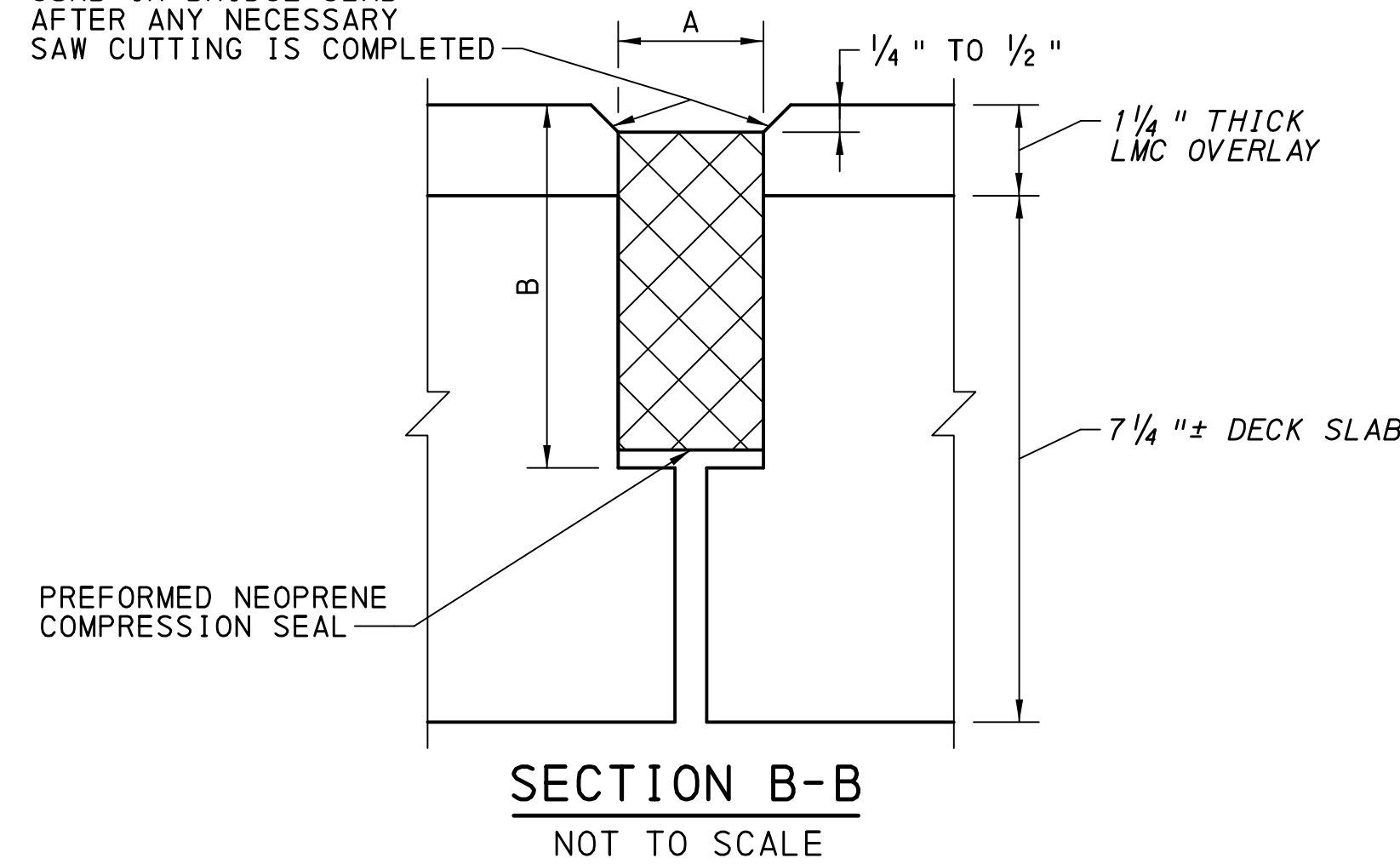
RECOMMENDED _____

SHEET 8 OF 10

S-

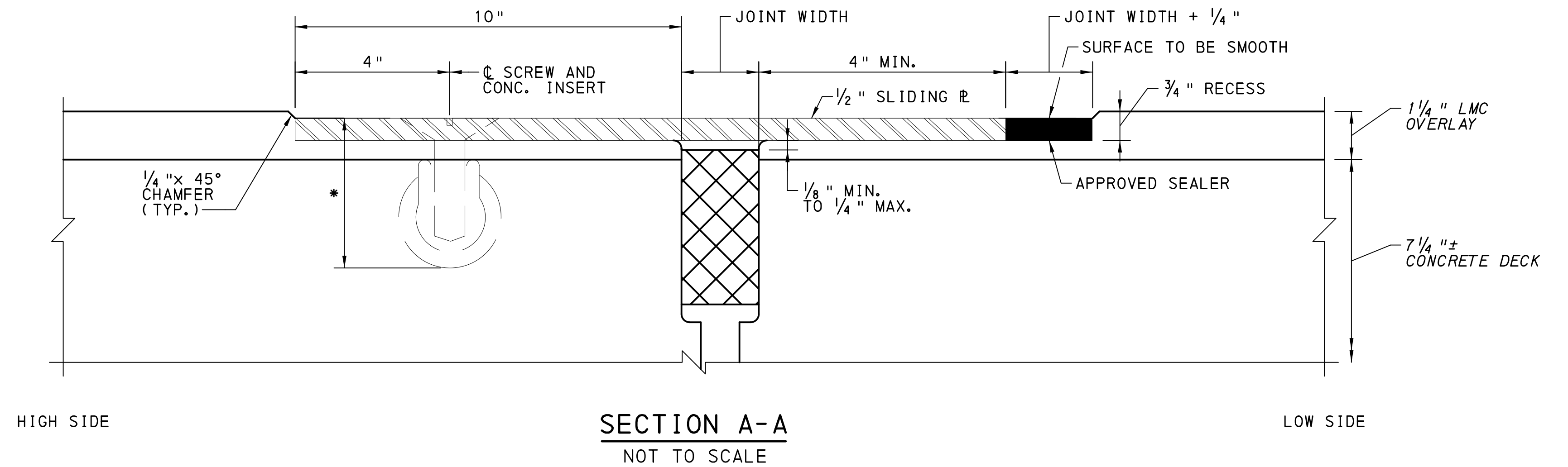
c:\pwork\proj\ecw\ise\1\schmi\d0273619_08 - Barr.r - P10n - 2.dgn 12/2/2019 8:03:50 AM 115-3333 fscmld

GRIND 1/8" TO 1/4" X 45° CHAMFER FROM CURB TO CURB IN BRIDGE SLAB AFTER ANY NECESSARY SAW CUTTING IS COMPLETED



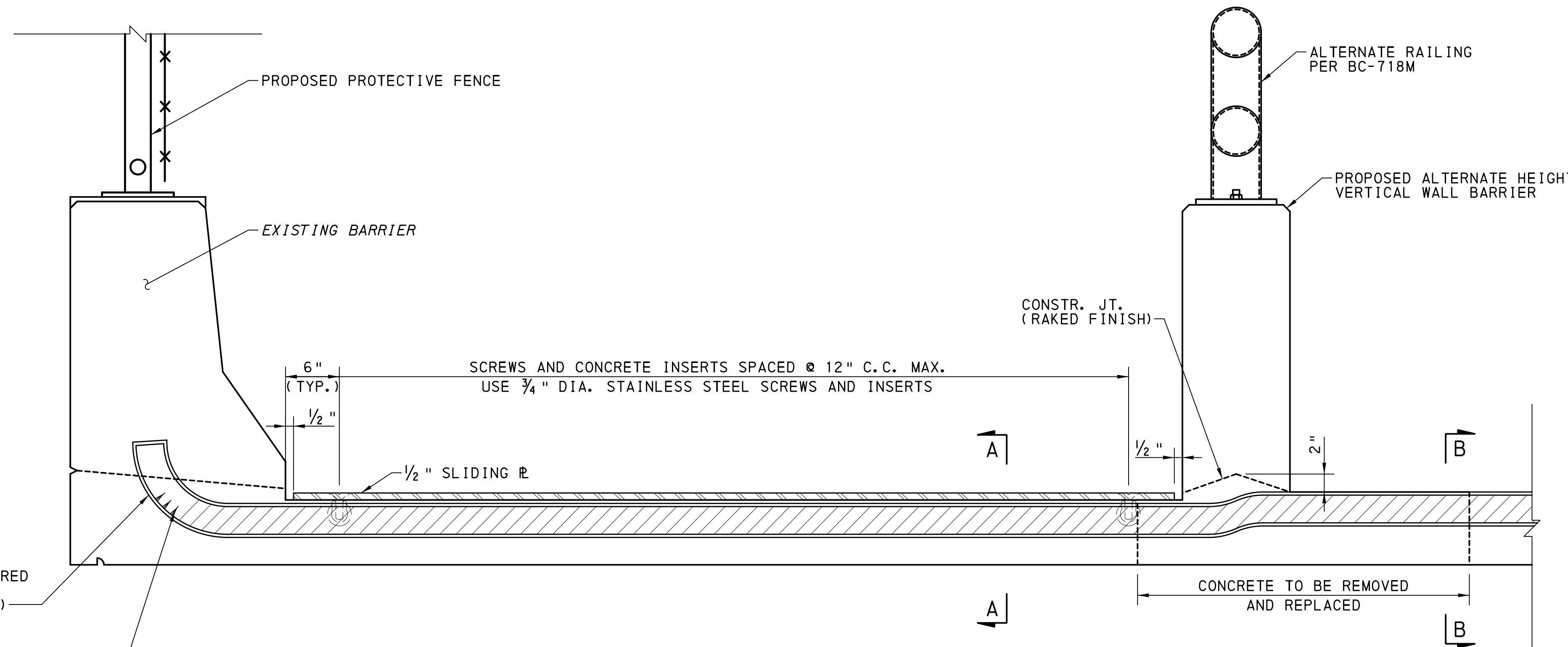
A = WIDTH OF SEAL GROOVE. SET PER MANUFACTURER'S RECOMMENDATIONS AND AMBIENT AIR TEMPERATURE AT TIME OF INSTALLATION. PROVIDE A CHART ON THE SHOP DRAWINGS FOR THE DIMENSION "A" FOR A TEMPERATURE RANGE OF 10° F TO 100° F.

B = MINIMUM DEPTH PER MANUFACTURER'S SPECIFICATIONS. SAW CUT THE REQUIRED DEPTH IF NEEDED. SAW CUTTING IS INCIDENTAL TO THE COST OF THE COMPRESSION SEAL.



* CONCRETE INSERTS TO BE ADHESIVE ANCHORS. DRILL DOWEL HOLES FOR ANCHOR INSERTS TO A DEPTH PER MANUFACTURER'S RECOMMENDATIONS.

NOTE: SAW CUT CONCRETE RECESS AREA IN SIDEWALK AND GRIND TO PROVIDE A SMOOTH SURFACE. APPLY ONE COAT OF ASPHALT CEMENT PAINT WA-1 OR PERFORMANCE GRADE ASPHALT PG 64-22 TO ALLOW SLIDING PLATE TO MOVE FREELY WITHOUT FRICTION. REPAIR DAMAGED CONCRETE BELOW RECESS OR AROUND DOWEL HOLES FOR CONCRETE INSERTS USING A NON-SHRINK GROUT CONFORMING TO PUB 408 SECTION 1001.2(d).



PIER JOINT DETAILS

NOT TO SCALE

NOTE: BEAMS AND REINFORCEMENT NOT SHOWN FOR CLARITY

NOTES

- FOR GENERAL NOTES, SEE SHEET 2.
- FOR BARRIER DETAILS, SEE SHEETS 7 AND 8.
- FOR ADDITIONAL NEOPRENE COMPRESSION SEAL AND SLIDING PLATE DETAILS, SEE STANDARD DRAWING BC-766M

Mark	Description	By	Chk' d.	Recm' d.	Date
REVISIONS					

COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION

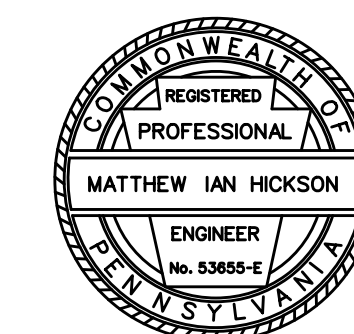
BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
JOINT DETAILS

RECOMMENDED _____

SHEET 9 OF 10

S-

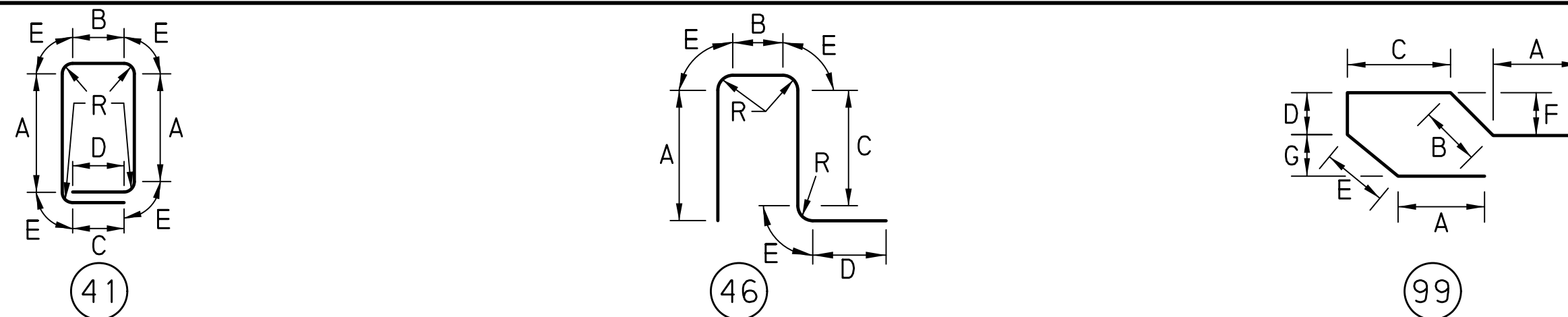
PREPARED BY
TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



REINFORCEMENT BAR SCHEDULE

MARK	NO. REQD	SIZE	TYPE	LENGTH	A	B	C	D	E	F	G	REMARKS
EC403	298	4	41	5'-9 1/2"	1'-9"	0'-3"	0'-5 1/2"	0'-3"	0'-4"			R=2"
EC406	5	4	41	5'-11" TO 7'-9 1/2"	1'-9"	0'-3 1/2" TO 0'-11"	0'-6" TO 1'-1 1/2"	0'-3 1/2" TO 0'-11"	0'-4"			VARY 1 EACH BY 5 5/8", B, C, AND D BY 1 7/8", R=2"
EC408	6	4	41	7'-9 1/2"	1'-9"	0'-11"	1'-1 1/2"	0'-11"	0'-4"			R=2"
EC410	6	4	41	5'-9 1/2" TO 7'-9 1/2"	1'-9"	0'-3" TO 0'-11"	0'-5 1/2" TO 1'-1 1/2"	0'-3" TO 0'-11"	0'-4"			VARY 2 EACH, B, C, AND D BY 4", R=2"
EC416	5	4	41	6'-2" TO 7'-9 1/2"	1'-9"	0'-4 1/2" TO 0'-11"	0'-7" TO 1'-1 1/2"	0'-4 1/2" TO 0'-11"	0'-4"			VARY 1 EACH BY 4 7/8", B, C, AND D BY 1 5/8", R=2"
EC505	2	5	99	15'-2"	3'-8"	0'-11 3/8"	4'-1"	0'-7 1/2"	2'-2 1/8"	0'-8"	0'-7 1/2"	
EC512	4	5	STR	41'-5"								
EC515	2	5	99	14'-3"	3'-8"	0'-11 3/8"	3'-8"	0'-8 1/2"	1'-7 1/8"	0'-8"	0'-6 1/2"	
EC522	16	5	STR	42'-2"								
EC532	4	5	STR	27'-11"								
EC605	2	6	99	16'-8"	4'-5"	0'-11 3/8"	4'-1"	0'-7 1/2"	2'-2 1/8"	0'-8"	0'-7 1/2"	
EC611	4	6	STR	41'-5"								
EC615	2	6	99	15'-9"	4'-5"	0'-11 3/8"	3'-8"	0'-8 1/2"	1'-7 1/8"	0'-8"	0'-6 1/2"	
EC621	16	6	STR	42'-5 1/2"								
EC631	4	6	STR	27'-11"								
ES404	298	4	46	6'-0 1/2"	1'-9"	0'-3"	1'-6 1/2"	1'-6"	0'-4"			R=2"
ES407	5	4	46	6'-1 1/8" TO 6'-8 5/8"	1'-9"	0'-3 5/8" TO 0'-11 1/8"	1'-6 1/2"	1'-6"	0'-4"			VARY 1 EACH, B BY 1 7/8" R=2"
ES409	6	4	46	6'-8 1/2"	1'-9"	0'-11"	1'-6 1/2"	1'-6"	0'-4"			R=2"
ES411	6	4	46	6'-0 1/2" TO 6'-8 1/2"	1'-9"	0'-3" TO 0'-11"	1'-6 1/2"	1'-6"	0'-4"			VARY 2 EACH, B BY 4" R=2"
ES417	5	4	46	6'-2" TO 6'-8 1/2"	1'-9"	0'-4 1/2" TO 0'-11"	1'-6 1/2"	1'-6"	0'-4"			VARY 1 EACH, B BY 1 5/8" R=2"

BENDING DIAGRAM



NOTES

- FOR GENERAL NOTES, SEE SHEET 2.
- FOR REINFORCEMENT BAR FABRICATION DETAILS, REFER TO STANDARD DRAWING BC-736M.
- ALL DIMENSIONS ARE OUT-TO-OUT OF BAR EXCEPT "R" WHICH IS SHOWN TO THE INSIDE OF THE BAR.
- EPOXY COATED BARS ARE DENOTED BY THE PREFIX "E".

Mark	Description	By	Chk' d.	Recm' d.	Date
REVISIONS					

**COMMONWEALTH OF PENNSYLVANIA
DEPARTMENT OF TRANSPORTATION**

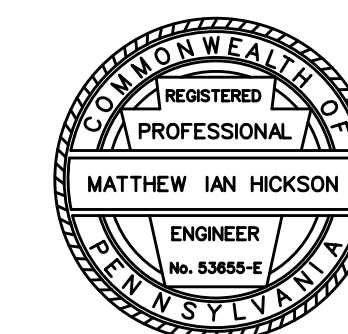
BUCKS COUNTY
PENNDOT DISTRICT 6-0
SR 1001 (S. MAIN STREET)
SEG 0030 OFFSET 0299
OVER SR 0202 BYPASS
PROTECTIVE BARRIER RETROFIT
REBAR SCHEDULE

RECOMMENDED _____

SHEET 10 OF 10

S-

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TRAFFIC PLANNING & DESIGN, INC.
4000 CRUMS MILL ROAD, SUITE 102
HARRISBURG, PENNSYLVANIA



South Main Street Trail plans

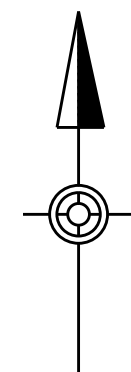
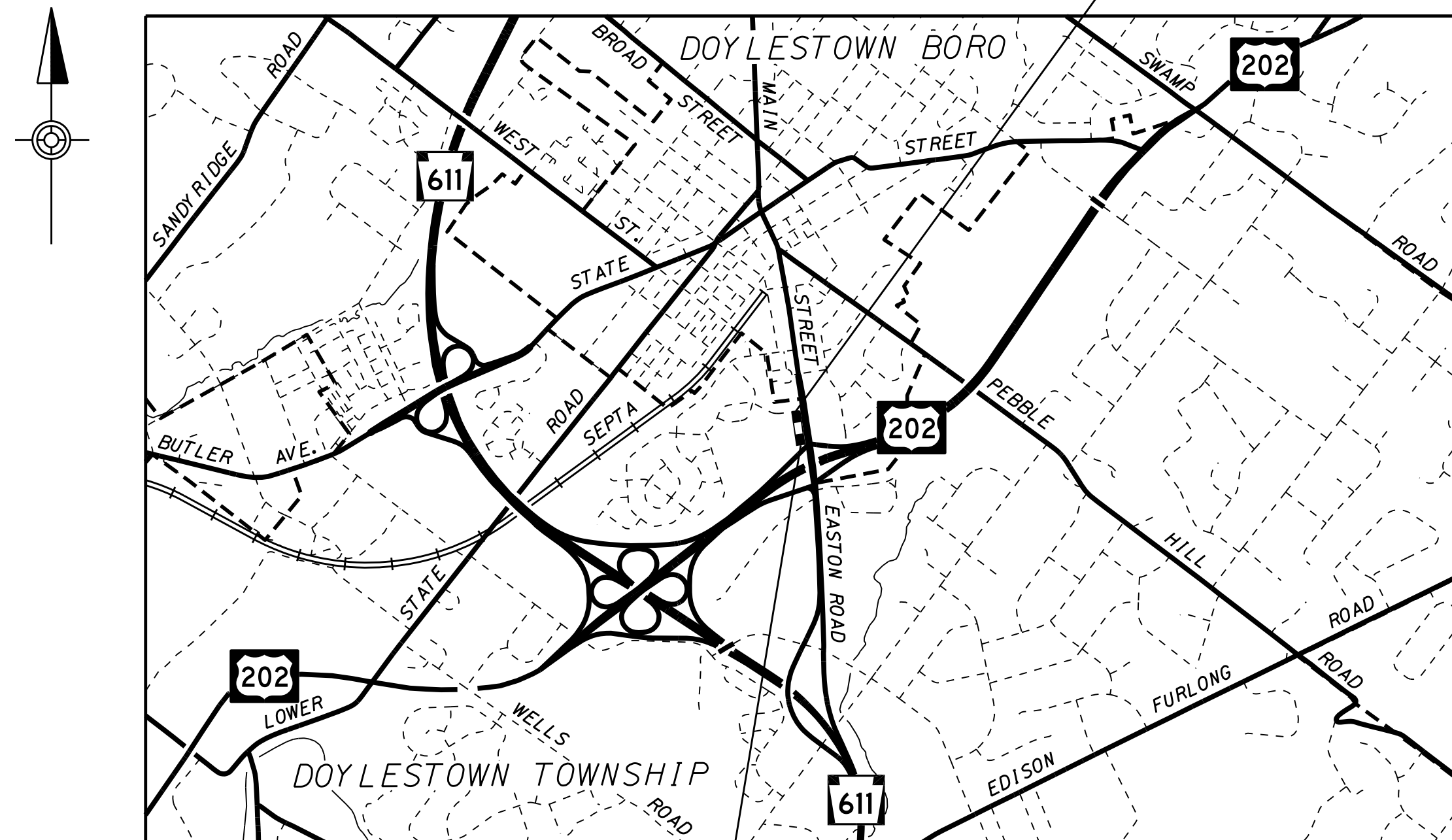
DOYLESTOWN TOWNSHIP

DRAWINGS FOR CONSTRUCTION OF STEEPLECHASE TRAIL

EASTON ROAD
DOYLESTOWN TOWNSHIP
BUCKS COUNTY, PA

LOCATION PLAN

LIMIT OF WORK
STA 16+11.95
SHARED USE PATH
EASTON ROAD
DOYLESTOWN TOWNSHIP
BUCKS COUNTY



LEGEND

	PROJECT
	STATE HIGHWAY
	STATE ROAD
	TOWNSHIP ROAD
	MUNICIPAL BOUNDARY

LIMIT OF WORK
STA 10+00.00
SHARED USE PATH
EASTON ROAD
DOYLESTOWN TOWNSHIP
BUCKS COUNTY

LIST OF PUBLIC UTILITIES

PECO ENERGY COMPANY
1050 W. SWEDESFORD ROAD
BERWYN, PA 19312
ATTN: WILLIAM HENSIL
PHONE: 610-725-7129

VERIZON PENNSYLVANIA INC.
180 SHEREE BLVD. SUITE 2100
EXTON PA, 19341
ATTN: VINCE PAWLICKI
PHONE: 215-591-6306

COMCAST CABLE COMMUNICATIONS
2320 TRENTON ROAD
LEVITTOWN, PA 19056
ATTN: RICHARD KAIN
PHONE: 610-633-1268

CALL BEFORE YOU DIG !

PENNSYLVANIA LAW REQUIRES
3 WORKING DAYS NOTICE FOR CONSTRUCTION PHASE
AND 10 WORKING DAYS IN DESIGN STAGE
BEFORE YOU DIG CALL
THE PA ONE CALL SYSTEM TELEPHONE NUMBER.



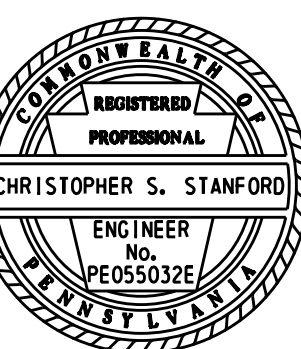
1-800-242-1776

PA ONE CALL SERIAL NUMBER
20200503058

SHEET INDEX	
DESCRIPTION	SHEET
TITLE SHEET	1
GENERAL NOTES & DETAILS	2
TYPICAL SECTIONS	3
CONSTRUCTION PLAN	4
SUPPLEMENTAL PLANS	
SIGNING AND PAVEMENT MARKING PLAN	1
EROSION AND SEDIMENTATION CONTROL PLAN	1

NUMBER REVISION	REVISIONS	DATE	BY

PLANS PREPARED BY:
MICHAEL BAKER INTERNATIONAL
500 Office Center Drive, Suite 210
Fort Washington, PA 19034
Phone: (215) 444-0888 · MBAKERINTL.COM



Michael Baker
INTERNATIONAL PROJECT MANAGER DATE

GENERAL NOTES:

THE LEGAL RIGHT-OF-WAY ON SR 1001 IS 120 FEET AS NOTED IN DRAWINGS RE-ESTABLISHING LIMITED ACCESS HIGHWAY AND AUTHORIZING CONDEMNATION OF RIGHT-OF-WAY, LR 1062 SECTION 1R/W, RECORDED AT BUCKS COUNTY RECORDER OF DEEDS ON AUGUST 23, 1968.

ALL REQUIRED RIGHT-OF-WAY FOR THIS PROJECT SHALL BE ACQUIRED IN FEE SIMPLE UNLESS OTHERWISE NOTED. AREAS, IF ANY, NOT TO BE ACQUIRED IN FEE SIMPLE SHALL BE ACQUIRED IN THE LESSER ESTATE OR INTEREST NOTED ON THE PLAN SHEET.

ALL CURVE DATA IS BASED ON THE ARC DEFINITION UNLESS OTHERWISE INDICATED.

ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).

HORIZONTAL CONTROL IS BASED ON THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM SOUTH ZONE, NORTH AMERICAN DATUM (NAD) OF 1983.

AVERAGE COMBINED SCALE FACTOR OF 0.99997298 WAS USED FOR THIS PROJECT.

DO NOT INTERFERE WITH THE OPERATION OF ANY FIRE HYDRANT, FIRE CALL BOX OR POLICE BOX.

SUMMARY OF PROJECT COORDINATES

SHEET 1 OF 1

BASED ON THE PENNSYLVANIA STATE PLANE COORDINATE SYSTEM (SOUTH ZONE)
NOTE: FOUR (4) PLACE COORDINATES ARE FOR COMPUTATIONAL PURPOSES ONLY
AND DO NOT IMPLY A PRECISION BEYOND TWO (2) DECIMAL PLACES.

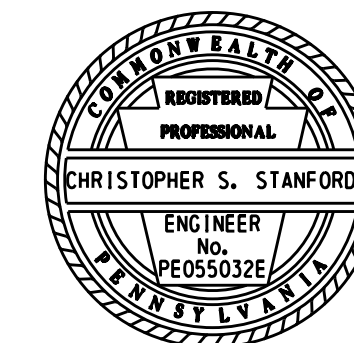
ROUTE	STATION	POINT	COORDINATES		BEARINGS
			NORTH	EAST	
SHARED USE PATH SURVEY & CONSTR	10+00.00	POB	362895.2519	2699655.6519	
	10+88.05	PC	362974.0170	2699616.2889	N 26° 33' 13" W
	11+12.41	PI	362995.8006	2699605.4025	
	11+35.11	PT	363019.7920	2699609.5800	N 9° 52' 39" E
	11+55.19	PC	363039.5783	2699613.0253	
	11+67.16	PI	363051.3714	2699615.0787	
	11+78.93	PT	363063.2104	2699613.3094	N 8° 30' 00" W
	12+09.38	PC	363093.3282	2699608.8083	
	12+17.19	PI	363101.0511	2699607.6541	
	12+24.98	PT	363108.8482	2699607.2299	N 4° 15' 47" W
	12+88.51	PC	363172.2005	2699602.5076	
	12+94.65	PI	363178.3241	2699602.0511	
	13+00.79	PT	363184.3972	2699601.1435	N 8° 30' 00" W
	13+63.33	PC	363246.2543	2699591.8989	
	13+76.97	PI	363259.7451	2699589.8826	
	13+90.31	PRC	363273.0681	2699592.8091	N 12° 23' 19" E
	14+03.95	PI	363286.3912	2699595.7356	
	14+17.29	PT	363299.8820	2699593.7193	N 8° 30' 00" W
	14+69.88	PC	363351.9008	2699585.9451	
	14+88.30	PI	363370.1097	2699583.2237	
15+05.97	PRC	363384.9205	2699572.2871	N 36° 26' 34" W	
15+31.00	PI	363405.0559	2699557.4188		
15+55.03	PT	363429.8213	2699553.7891	N 8° 20' 17" W	
16+11.95	POE	363486.1465	2699545.5339		

LEGEND:

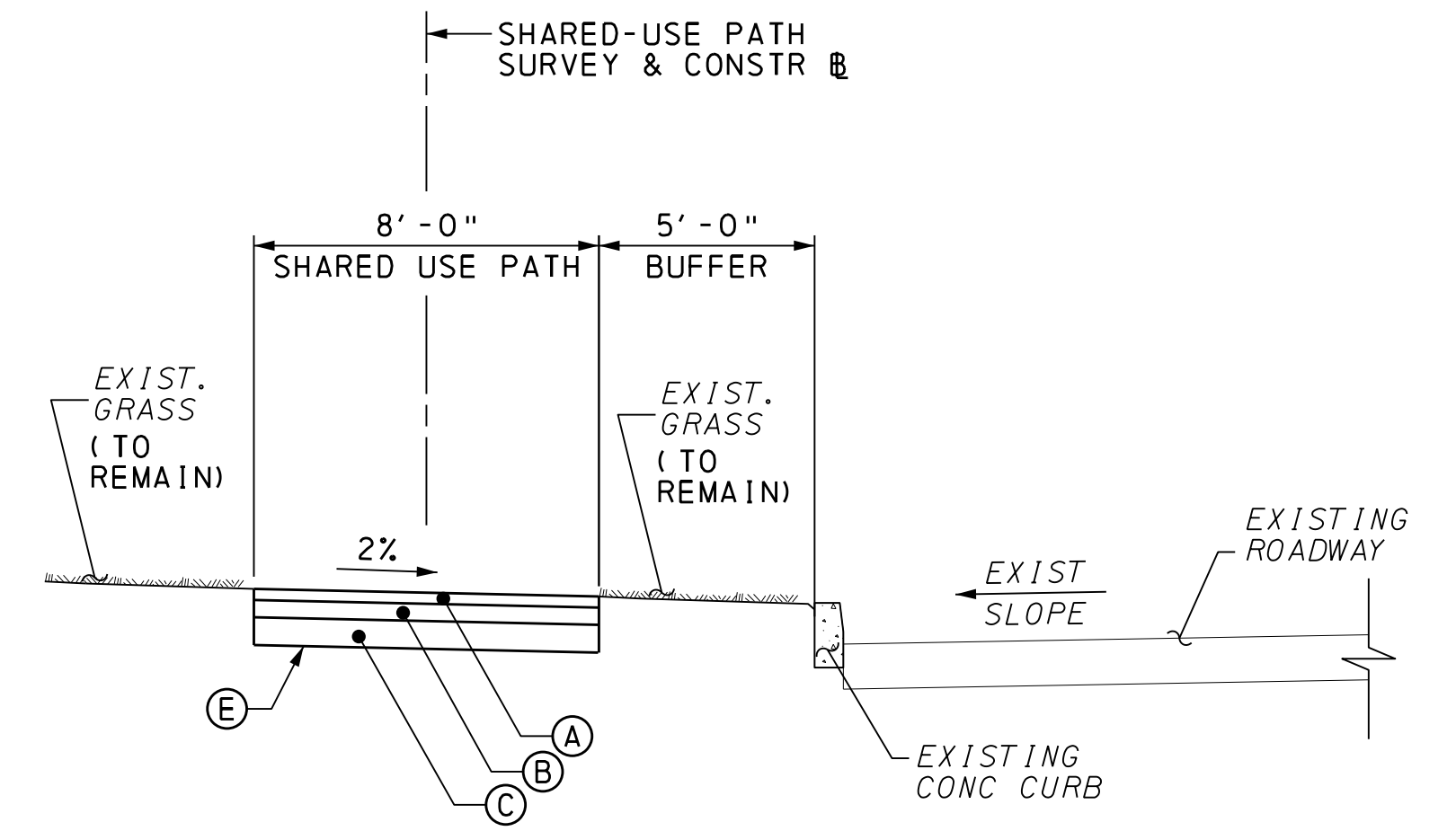
- POB = POINT OF BEGINNING
- PC = POINT OF CURVATURE
- PI = POINT OF INTERSECTION
- PT = POINT OF TANGENT
- PRC = POINT OF REVERSE CURVE
- POE = POINT OF END

REVISION NUMBER	REVISIONS	DATE	BY

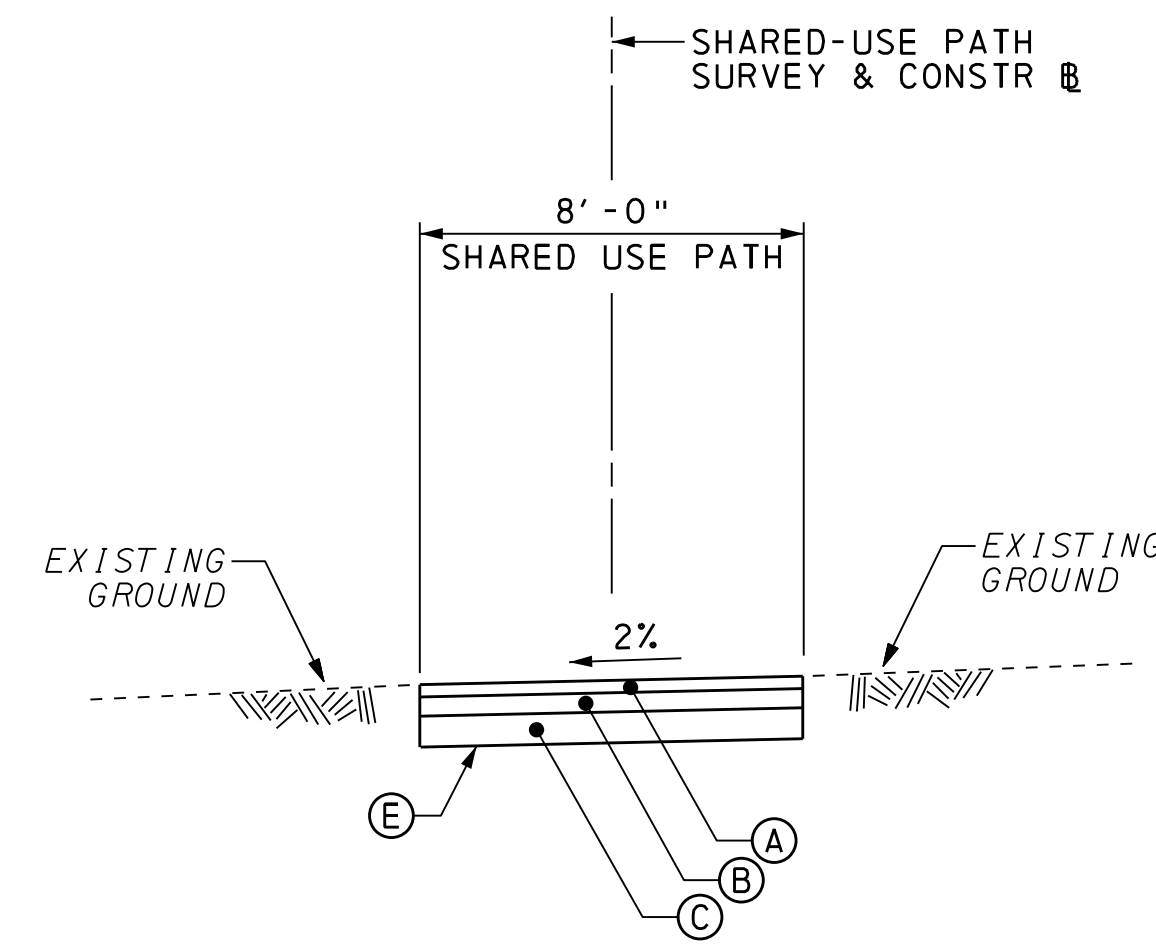
Michael Baker
INTERNATIONAL
500 OFFICE CENTER DRIVE, SUITE 210,
FORT WASHINGTON, PA 19034-3234
Phone: (215) 444-0888 · MBAKERINTL.COM



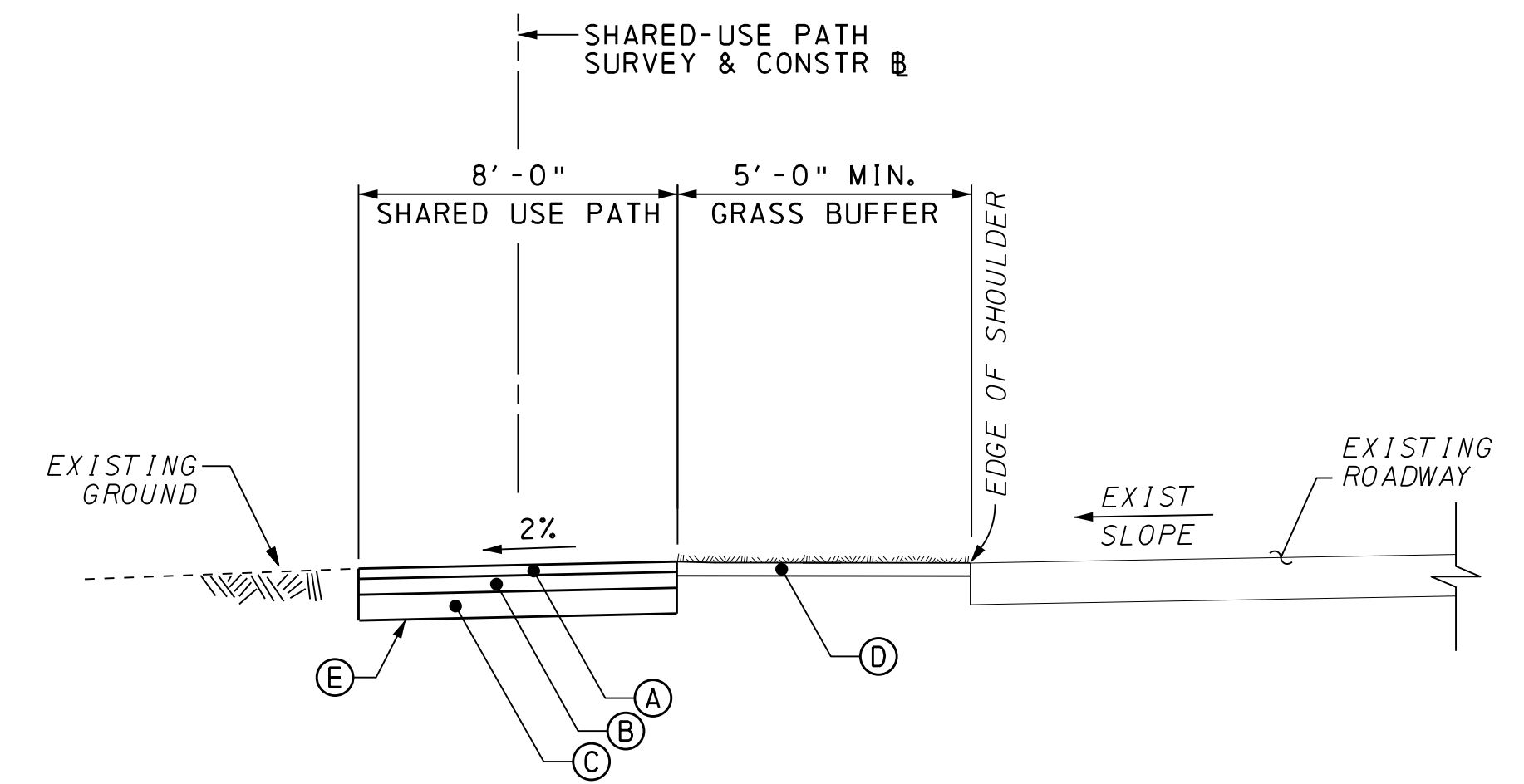
STEEPLECHASE TRAIL DOYLESTOWN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA		
DOYLESTOWN TOWNSHIP 425 WELLS ROAD DOYLESTOWN, PA 18901		
DATE 7/15/2020	CONSTRUCTION PLANS	APPROVED CSS
DRAWN BY ZMS	DETAILS /GENERAL NOTES	SHEET 2 OF 4
ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.		



TYPICAL SECTION
NOT TO SCALE
STA 14+25 TO STA 15+48



TYPICAL SECTION
NOT TO SCALE
STA 10+35 TO STA 11+17



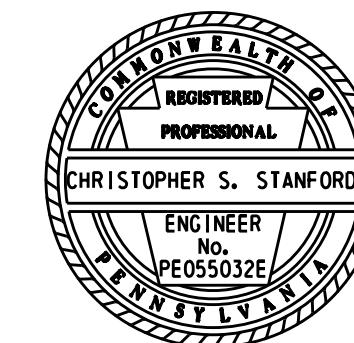
TYPICAL SECTION
NOT TO SCALE
STA 11+90 TO STA 14+25

LEGEND

- (A) SUPERPAVE ASPHALT MIXTURE DESIGN, HMA WEARING COURSE, PG 64-22, 0.3 TO <3 MILLION ESALS, 9.5 mm MIX, 1 1/2" DEPTH, SRL-H
- (B) SUPERPAVE ASPHALT MIXTURE DESIGN, HMA BASE COURSE, PG 64-22, 0.3 TO <3 MILLION ESALS, 19.0 mm MIX, 3" DEPTH
- (C) SUBBASE 6" DEPTH (NO. 2A)
- (D) 4" TOPSOIL
- (E) GEOTEXTILE, CLASS 4, TYPE A

REVISION NUMBER	REVISIONS	DATE	BY

Michael Baker INTERNATIONAL
500 OFFICE CENTER DRIVE, SUITE 210,
FORT WASHINGTON, PA 19034-3234
Phone: (215) 444-0888 • MBAKERINTL.COM



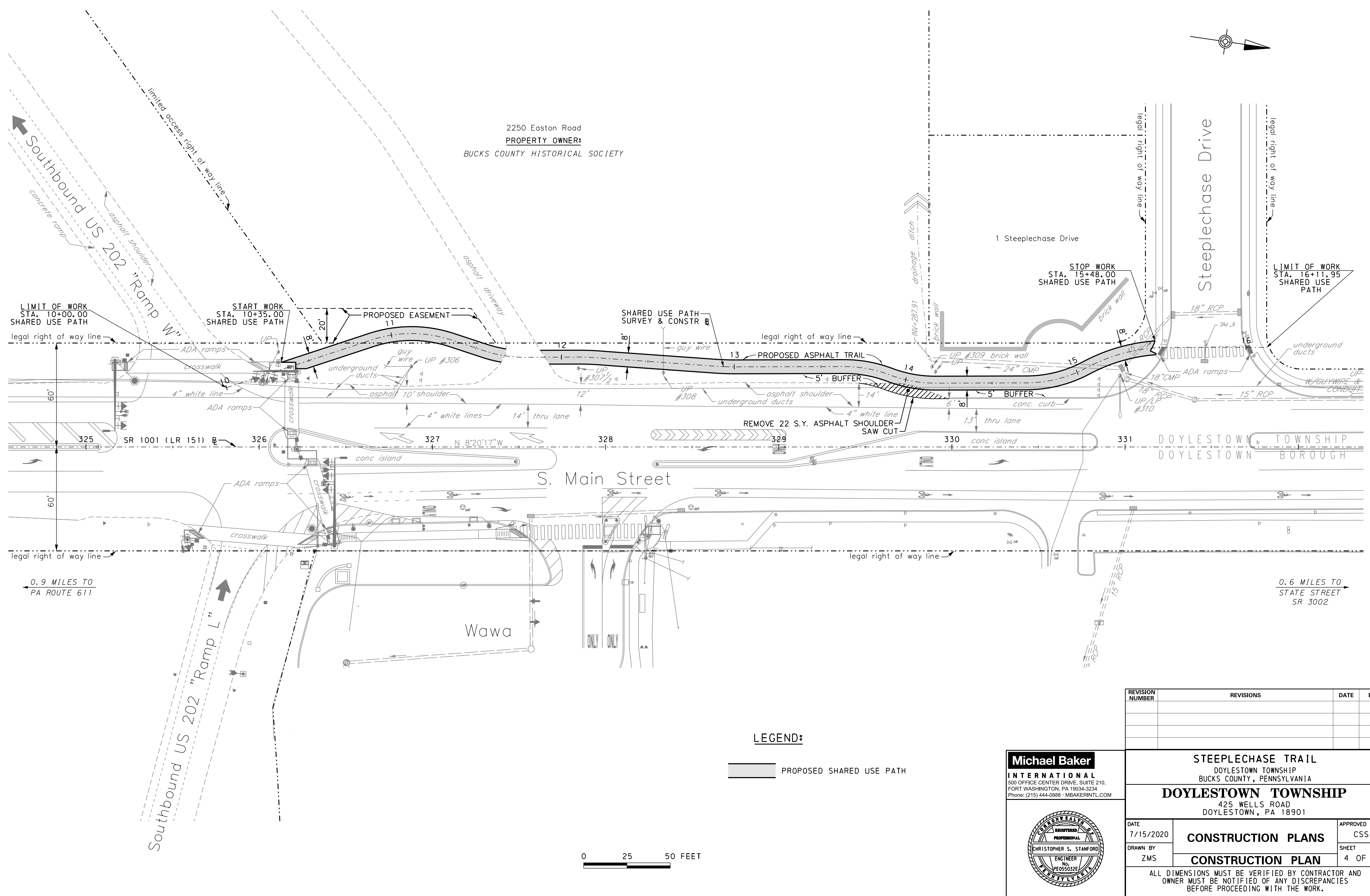
STEEPLECHASE TRAIL
DOYLESTOWN TOWNSHIP
BUCKS COUNTY, PENNSYLVANIA
DOYLESTOWN TOWNSHIP
425 WELLS ROAD
DOYLESTOWN, PA 18901

DATE 7/14/2020	CONSTRUCTION PLANS	APPROVED CSS
DRAWN BY ZMS		SHEET 3 OF 4

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.

16-JUL-2020

E:\161695_Perkiomen_Crossing_Improvement\Working\ZMS\SCT_50_scale_plan_w_NONPenDOT_border.dgn



LEGEND:

PROPOSED SHARED USE PATH



Michael Baker International
500 OFFICE CENTER DRIVE, SUITE 210,
FORT WASHINGTON, PA 19034-3234
Phone: (215) 444-0888 · MBAKERINTL.COM

REVISION NUMBER	REVISIONS	DATE	BY

STEEPLECHASE TRAIL
DOYLESTOWN TOWNSHIP
BUCKS COUNTY, PENNSYLVANIA

DOYLESTOWN TOWNSHIP
425 WELLS ROAD
DOYLESTOWN, PA 18901

DATE 7/15/2020	APPROVED CSS
DRAWN BY ZMS	SHEET 4 OF 4

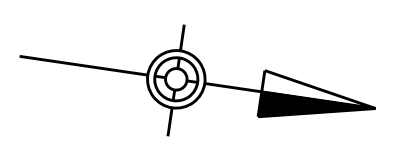
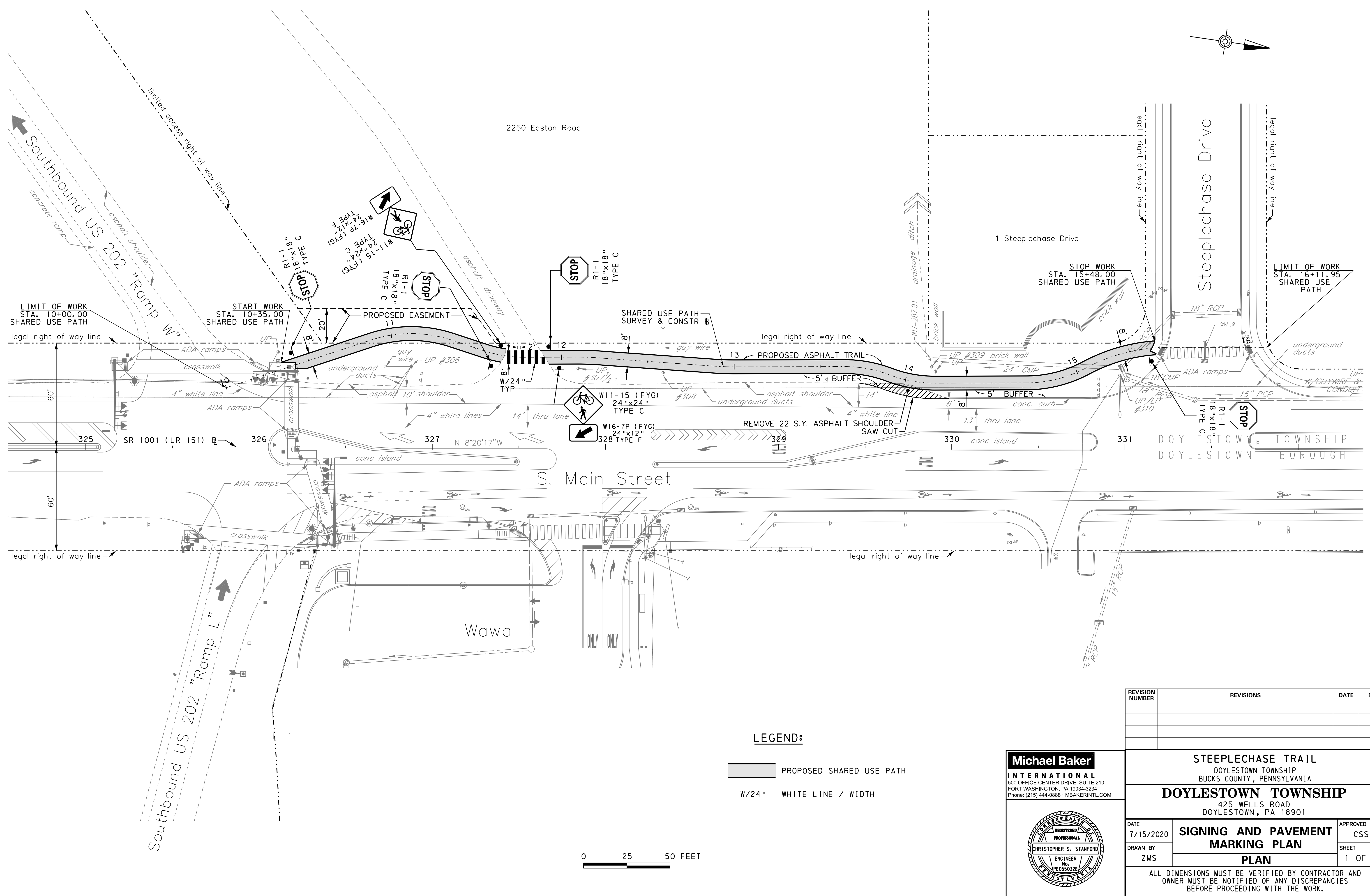
CONSTRUCTION PLANS

CONSTRUCTION PLAN

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.

16-JUL-2020

E:\161695_Perkiomen_Crossing_Improvement\Work\ZMS\SCT_SIGNING_PAVT_MARKING_PLAN_w_border.dgn



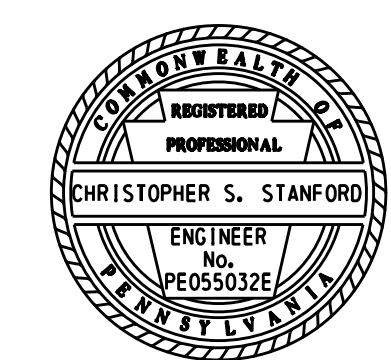
LEGEND:

- PROPOSED SHARED USE PATH
- W/24" WHITE LINE / WIDTH



REVISION NUMBER	REVISIONS	DATE	BY

Michael Baker INTERNATIONAL
 500 OFFICE CENTER DRIVE, SUITE 210,
 FORT WASHINGTON, PA 19034-3234
 Phone: (215) 444-0888 · MBAKERINTL.COM



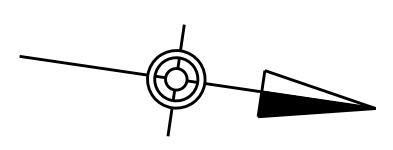
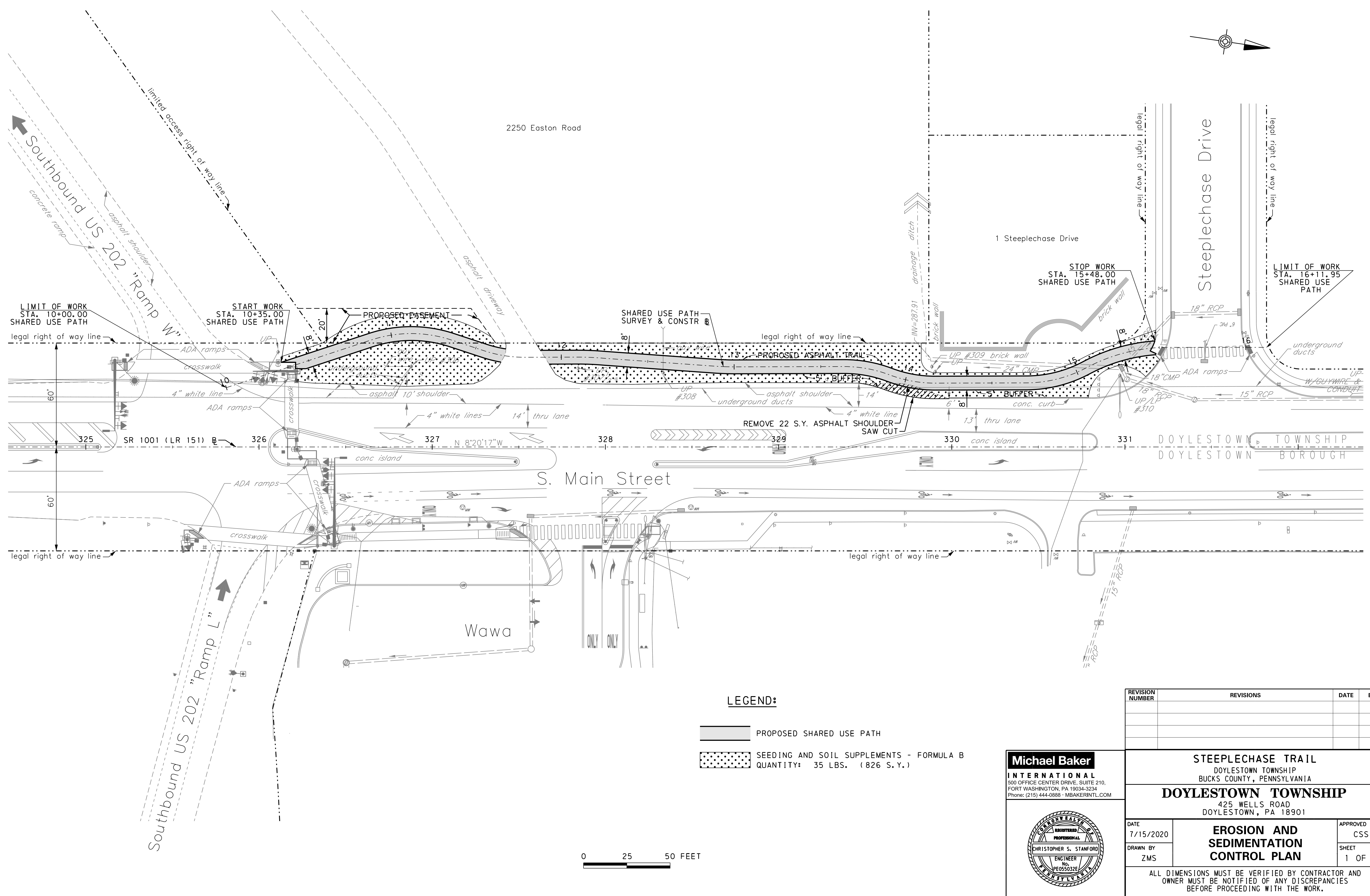
STEEPLECHASE TRAIL
 DOYLESTOWN TOWNSHIP
 BUCKS COUNTY, PENNSYLVANIA

DOYLESTOWN TOWNSHIP
 425 WELLS ROAD
 DOYLESTOWN, PA 18901

DATE 7/15/2020	SIGNING AND PAVEMENT MARKING PLAN	APPROVED CSS
DRAWN BY ZMS		SHEET 1 OF 1

PLAN

ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.

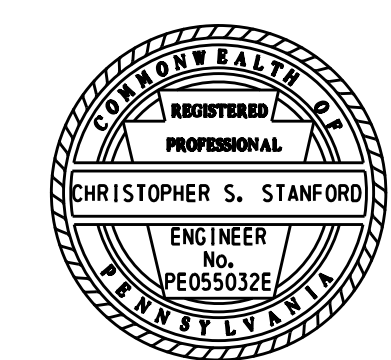


LEGEND:

- PROPOSED SHARED USE PATH
- SEEDING AND SOIL SUPPLEMENTS - FORMULA B
QUANTITY: 35 LBS. (826 S.Y.)



Michael Baker International
 500 OFFICE CENTER DRIVE, SUITE 210,
 FORT WASHINGTON, PA 19034-3234
 Phone: (215) 444-0888 · MBAKERINTL.COM



REVISION NUMBER	REVISIONS	DATE	BY


STEEPLECHASE TRAIL		
DOYLESTOWN TOWNSHIP BUCKS COUNTY, PENNSYLVANIA		
DOYLESTOWN TOWNSHIP		
425 WELLS ROAD DOYLESTOWN, PA 18901		
DATE 7/15/2020	EROSION AND SEDIMENTATION CONTROL PLAN	APPROVED CSS
DRAWN BY ZMS		SHEET 1 OF 1
ALL DIMENSIONS MUST BE VERIFIED BY CONTRACTOR AND OWNER MUST BE NOTIFIED OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.		

Alternatives Analysis Plan

10-FEB-2021

ES:\166816_Turkey_Trail_Study\PIA\01\Map\Boroughs\SR-313\Aerial\1-1_Plan.dwg







PREPARED FOR:
 **DOYLESTOWN TOWNSHIP**

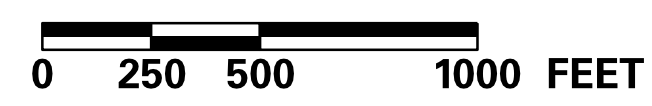
PREPARED BY:
Michael Baker INTERNATIONAL
 500 OFFICE CENTER DRIVE SUITE 210
 FORT WASHINGTON, PA 19034

SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
 TRAIL FEASIBILITY STUDY
 DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

-  PROPOSED SHARED USE PATH/TRAIL - RED ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - YELLOW ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - BLUE ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - BLACK ROUTE

-  PLANNED TRAILS
-  EXISTING TRAIL



Preferred Alternative Plan



PREPARED FOR:
 PREPARED BY:
Michael Baker
 INTERNATIONAL
 500 OFFICE CENTER DRIVE SUITE 210
 FORT WASHINGTON, PA 19034

SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
 TRAIL FEASIBILITY STUDY
 DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:
 PREFERRED SHARED USE PATH/TRAIL
 PLANNED TRAILS
 EXISTING TRAIL



E:\186816_TurkRd_Tr01_Study\Plan\Map\Boards\RT-313\011313_Preferrd Route_A11_Plan.dgn 21-JUL-2021

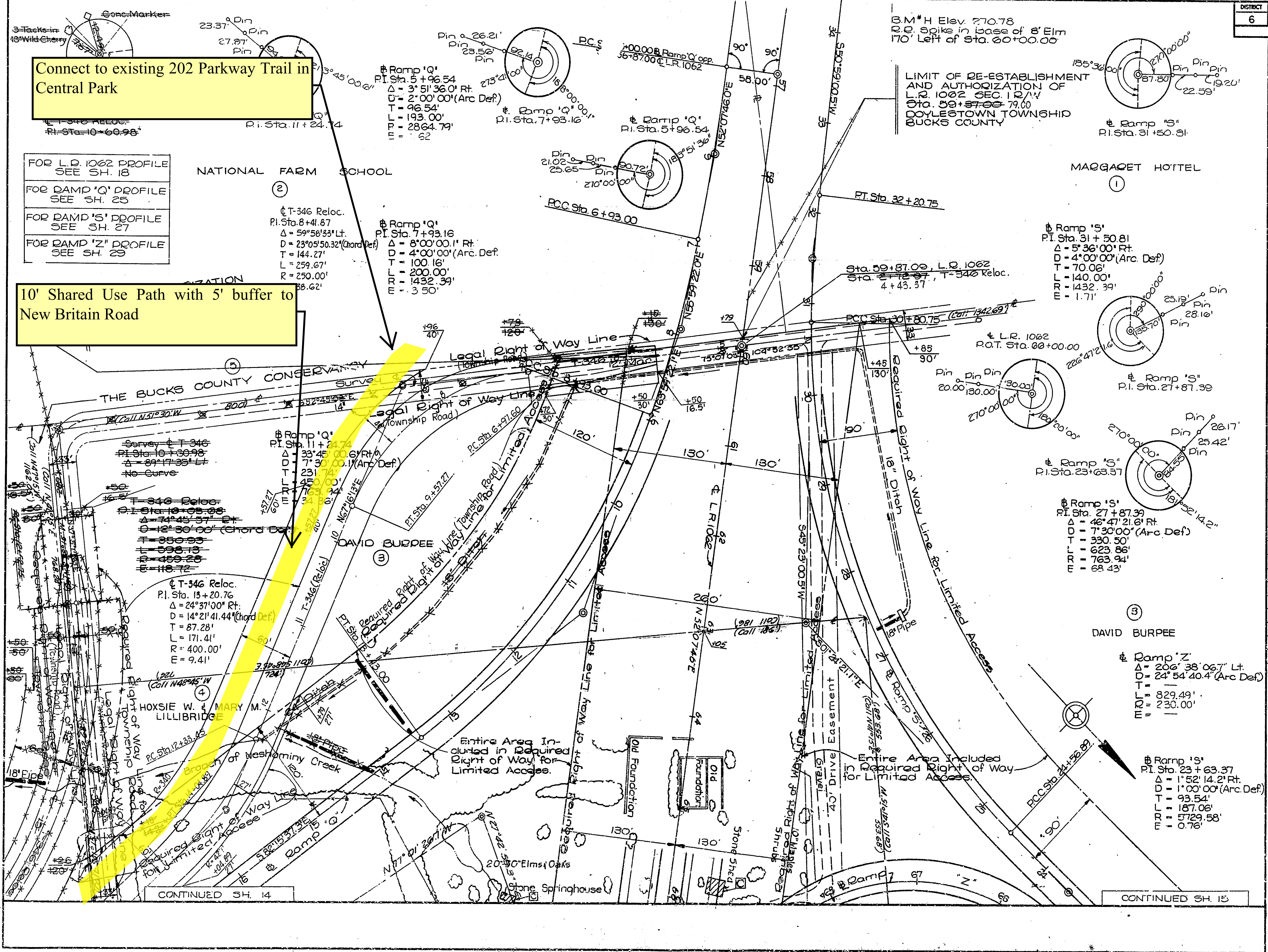
Preferred Alternative on Existing SR 202 Bypass Plans

DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	1R/W	10 OF 52
DOYLESTOWN TOWNSHIP				

Connect to existing 202 Parkway Trail in Central Park

- FOR L.R. 1062 PROFILE SEE SH. 18
- FOR RAMP 'Q' PROFILE SEE SH. 25
- FOR RAMP 'S' PROFILE SEE SH. 27
- FOR RAMP 'Z' PROFILE SEE SH. 29

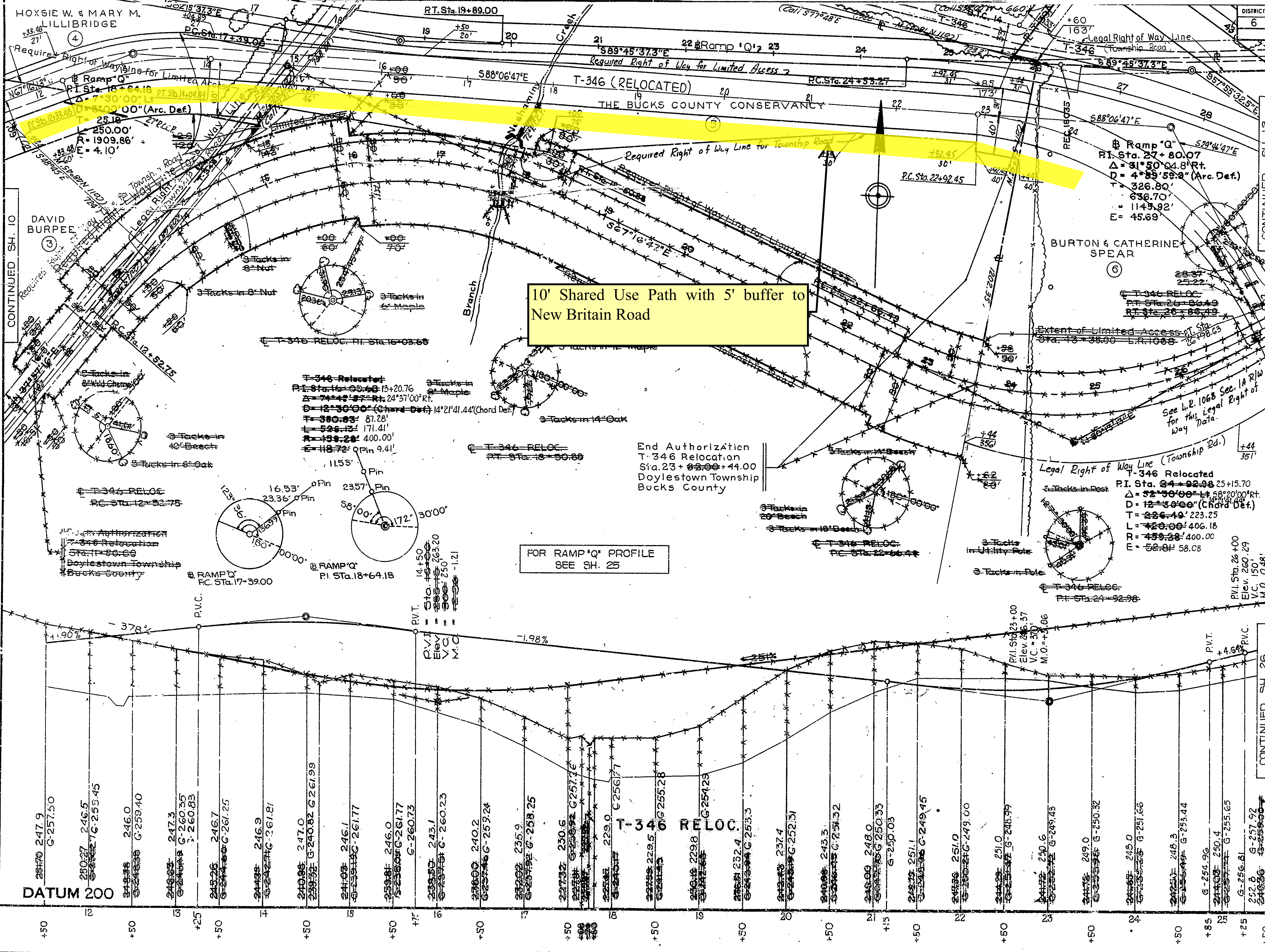
10' Shared Use Path with 5' buffer to New Britain Road



Parcel No.	Area Taken	Residue
	R W	Rt LI
1	SEE SHEET NO. 9	
2	SEE SHEET NO. 9	
3	83.73 Ac.	20.193 Ac. 166.810 Ac.
4	7636 S.F.	TEMP EASEMENT AR. 0
5	SEE SHEET NO. 14	

Parcel No.	Deed Book	Page
1	SHEET NO. 9	
2	SHEET NO. 9	
3	388	34E
3	559	38
3	587	476
3	595	345
3	774	572
3	1666	590
4	1217	343
5	SHEET NO. 14	

Alignment L.R. 1062 FB 48270
 Alignment Ramps FB 48272
 Topo L.R. 1062 FB 48273
 Topo Ramps FB 48277
 Bench Levels FB 48283
 Ref Ties FB 48284
 T-346 FB 48286



10' Shared Use Path with 5' buffer to New Britain Road

FOR RAMP 'Q' PROFILE SEE SH. 25

End Authorization
T-346 Relocation
Sta. 23+00.00 to 44.00
Doylestown Township
Bucks County

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6	BUCKS	1062	R/W	14 OF 52

Parcel No.	Area Taken		Residue	
	R/W	Rt	Lt	
3	SEE SHEET NO.10			
4	SEE SHEET NO.10			
5	3.044 Ac.	18.916 Ac.		0
6	SEE SHEET NO.12			
7	SEE SHEET NO.13			

Parcel No.	Deed Book	Page
3	SEE SHEET NO.10	
4	SEE SHEET NO.10	
5	1884	417
6	SEE SHEET NO.12	
7	SEE SHEET NO.13	

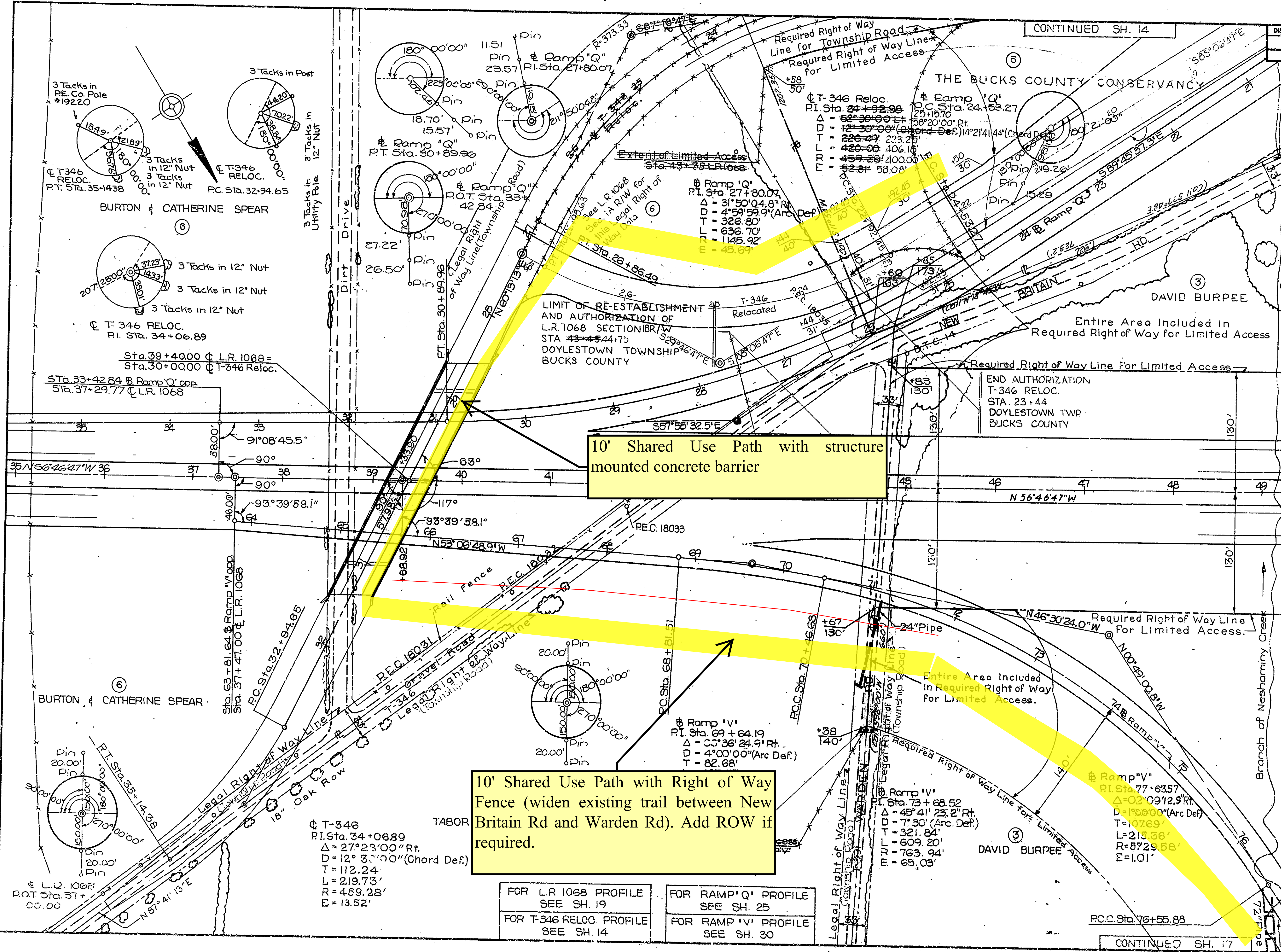
Alignment Ramp 'Q' F.B. 48272
 Topo Ramp 'Q' F.B. 48277
 Bench Levels Ramp 'Q' F.B. 48282
 Reference Ties Ramp 'Q' F.B. 48284
 T-346 F.B. 48286

DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	1R/W	13 OF 52
DOYLESTOWN TOWNSHIP				

Parcel No	Area Taken		Residue	
	R W	Rt.	Rt.	Li.
3	SEE SHEET NO. 10			
5	SEE SHEET NO. 14			
6	SEE SHEET NO. 12			
7	NO TAKE			

Parcel No.	Deed Book	Page
3	SHEET NO. 10	
5	SHEET NO. 14	
6	SHEET NO. 12	
7	377	560

Alignment L.R. 1068	F.B. 48271
Alignment Ramps	F.B. 48272
Topo L.R. 1068	F.B. 48288
Topo Ramps	F.B. 48277
Bench Levels	F.B. 48233
Reference Ties	F.B. 46284
T-346	F.B. 48286

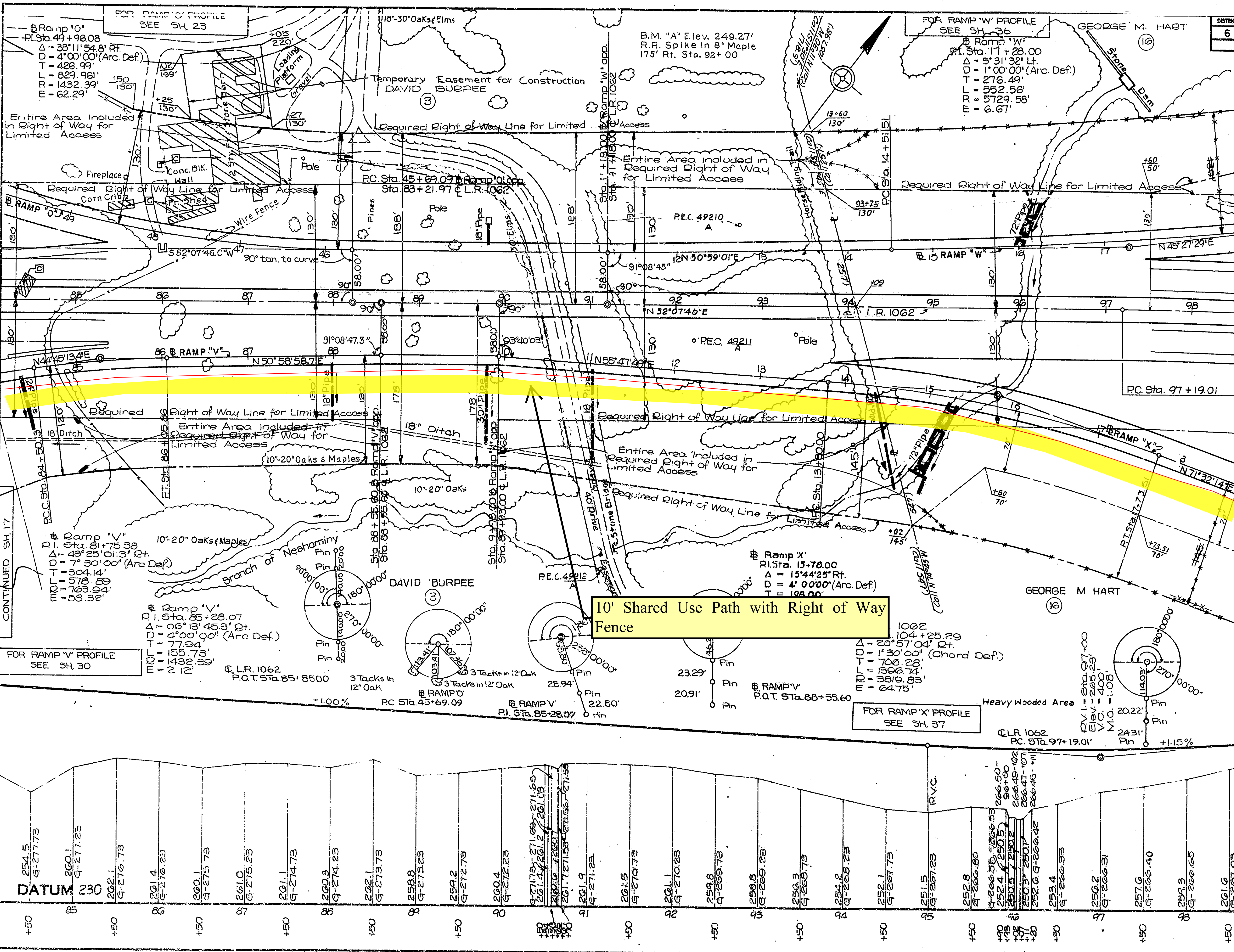


10' Shared Use Path with Right of Way Fence (widen existing trail between New Britain Rd and Warden Rd). Add ROW if required.

10' Shared Use Path with structure mounted concrete barrier

FOR L.R. 1068 PROFILE SEE SH. 19
FOR T-346 RELOC. PROFILE SEE SH. 14

FOR RAMP 'Q' PROFILE SEE SH. 25
FOR RAMP 'V' PROFILE SEE SH. 30



DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	1R/W	31 OF 52

DOYLESTOWN TOWNSHIP

Parcel No	Area Taken RW	Residue	
		Rt	Lt
3	SHEET NO.10		
16	46.98 Ac.	2.920 Ac.	22.143 Ac.
12.529 Ac.		4.410 Ac.	24.224 Ac.

Parcel No	Deed Book	Page
3	SHEET NO.10	
16	1577	584

10' Shared Use Path with Right of Way Fence

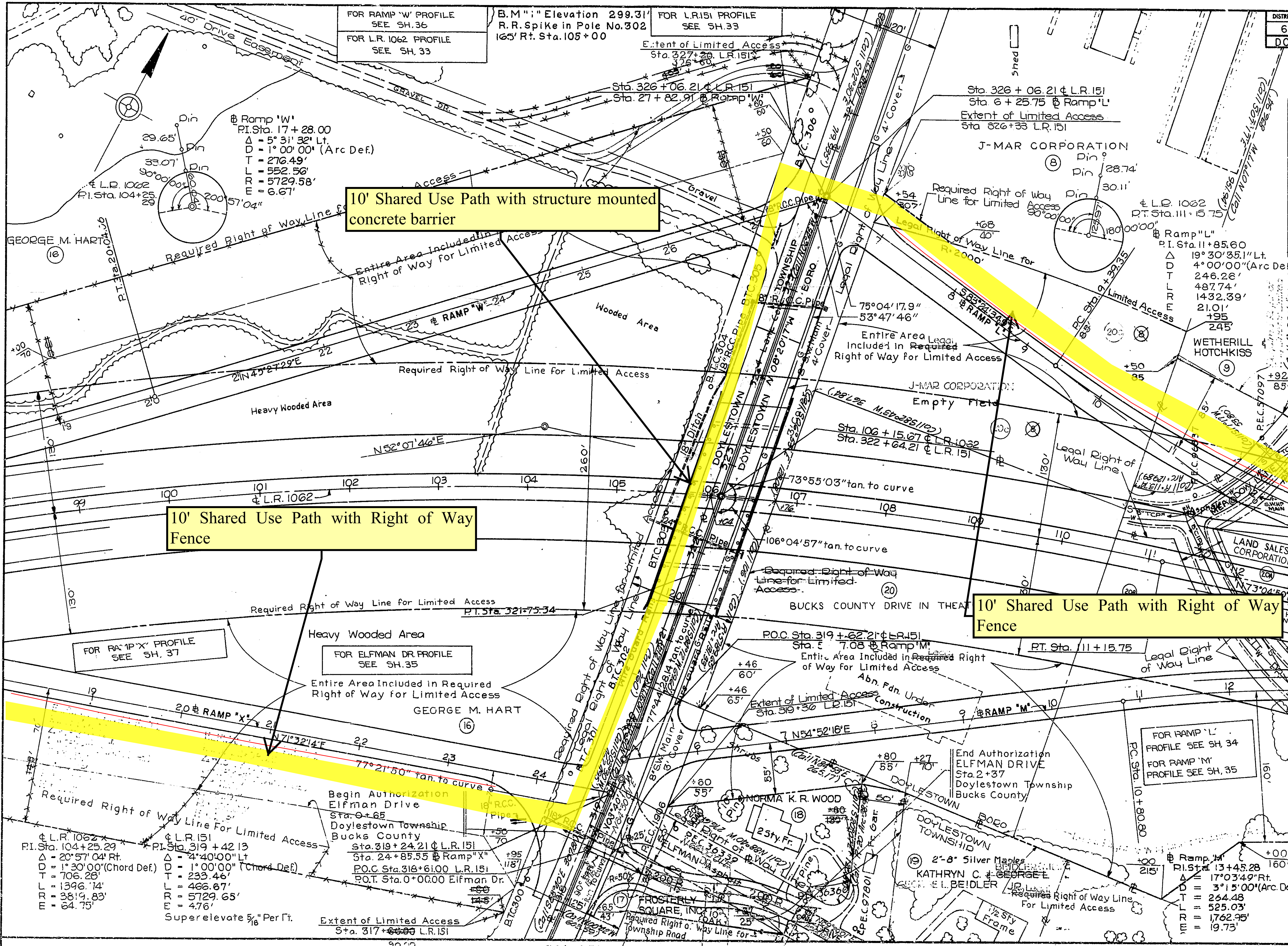
Alignment L.R.1062 F.B.48270
 Alignment Ramps "O" & "V" F.B.48282
 Topo L.R.1062 F.B.48273
 Topo Ramps "O" & "V" F.B.48277
 Topo Ramps "W" & "X" F.B.48276
 Bench Levels F.B.48283
 Ref. Ties F.B.48284

Parcel No.	Area Taken		Residue	
	R	W	Rt.	Lt.
8	.034	Ac.	0	6205 Ac.
9	.303	Ac.	0	3150 Ac.
16	SEE SHEET NO. 31			
17	.131	Ac.	3405	0
18	11660	S.F.	15649	S.F.
19	NO TAKE			
20	NO TAKE			
20A	NO TAKE			
20B	NO TAKE			

* Present Owner - Commonwealth of Pennsylvania Department of Highways

Parcel No.	Deed Book	Page
8	1678	346
9	1789	1088
16	SHEET NO. 31	
17	1627	451
18	1548	30
19	1642	19
20	1794	27
20A	1794	32
20B	1837	682

Alignment	L.R. 1062	F.B. 48270
Alignment	L.R. 151 Ramps 'L' & 'M'	F.B. 48271
Alignment	Ramps 'U' & 'X'	F.B. 48272
Topo	L.R. 1062	F.B. 48273
Topo	L.R. 151	F.B. 48275
Topo	Ramps	F.B. 48276
Bench Levels		F.B. 48283
Reference Ties		F.P. 48284



FOR RAMP 'W' PROFILE SEE SH. 36
FOR L.R. 1062 PROFILE SEE SH. 33

B.M. "E" Elevation 299.31
R.R. Spike in Pole No. 302
165' Rt. Sta. 105+00

FOR L.R. 151 PROFILE SEE SH. 33

10' Shared Use Path with Right of Way Fence

10' Shared Use Path with structure mounted concrete barrier

10' Shared Use Path with Right of Way Fence

FOR RAMP 'X' PROFILE SEE SH. 37

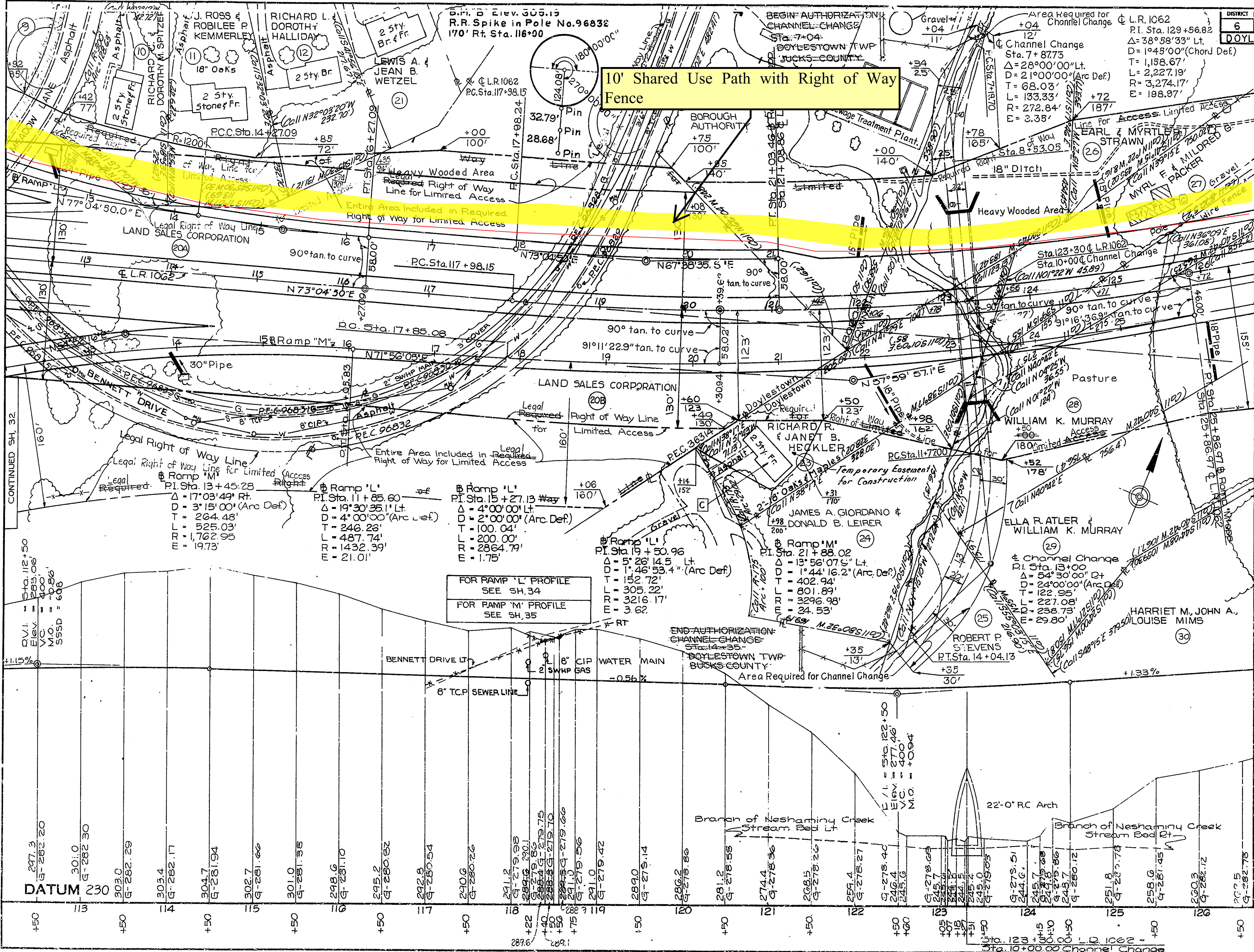
FOR ELFMAN DR PROFILE SEE SH. 35

FOR RAMP 'L' PROFILE SEE SH. 34
FOR RAMP 'M' PROFILE SEE SH. 35

FOR L.R. 1062
P.I. Sta. 104+25.29
Δ = 2° 57' 04" Rt.
D = 1° 30' 00" (Chord Def.)
T = 706.28'
L = 1396.74'
R = 3819.83'
E = 64.75'

FOR L.R. 151
P.I. Sta. 319+42.13
Δ = 4° 40' 00" Lt.
D = 1° 00' 00" (Chord Def.)
T = 233.46'
L = 466.67'
R = 5729.65'
E = 4.76'

FOR RAMP 'M'
P.I. Sta. 113+45.28
Δ = 17° 03' 49" Rt.
D = 3° 15' 00" (Arc Def.)
T = 264.48'
L = 525.03'
R = 1762.95'
E = 19.75'



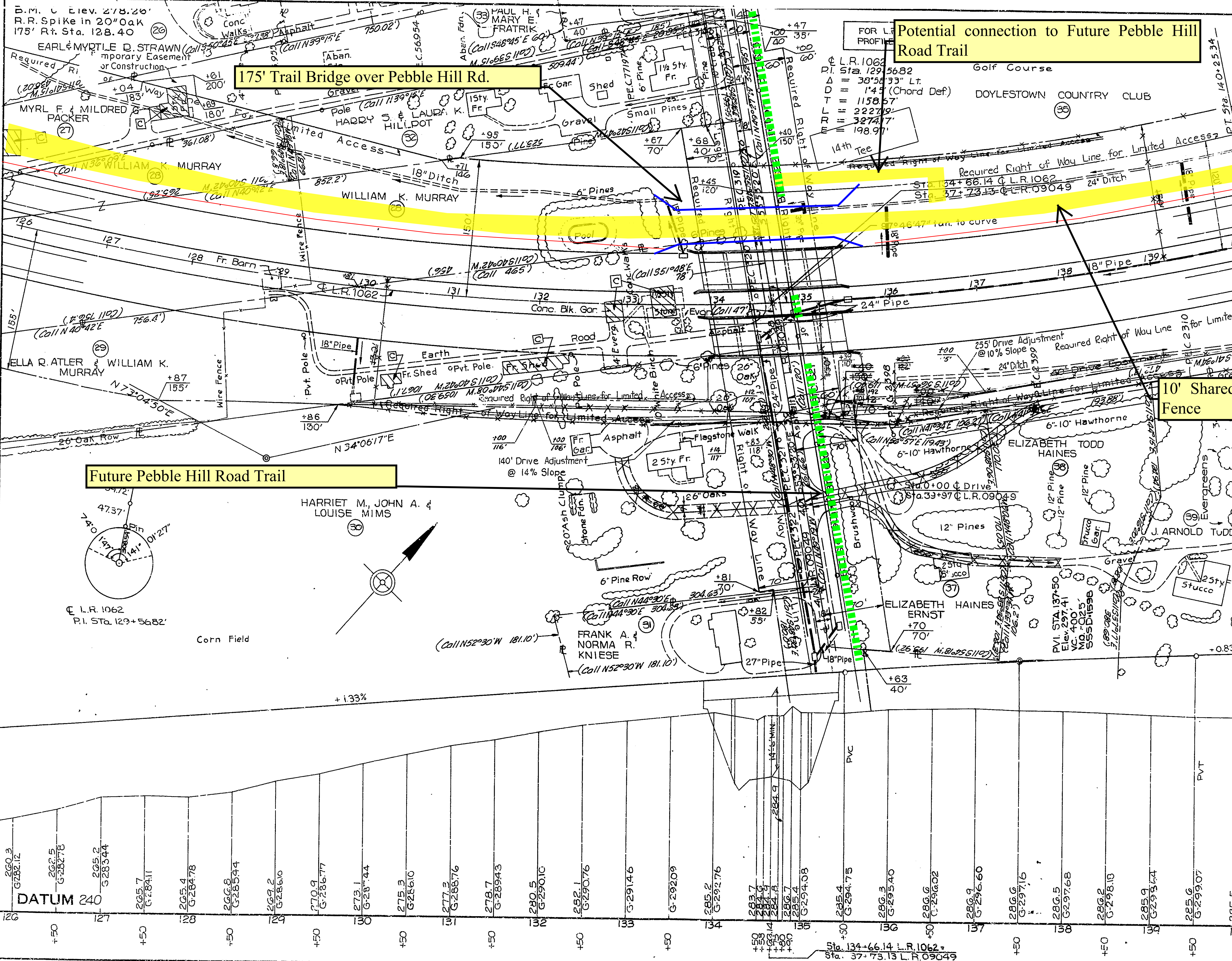
DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	R/W	38 OF 52
DOYLESTOWN TWP & DOYLESTOWN BORO				

Parcel No.	Area Taken		Residue	
	R W	Rt.	Rt.	Lt.
9	SEE SHEET NO. 32			
10	7231 S.F.	0	14854 S.F.	
11	2552 S.F.	0	24368 S.F.	
12	7849 S.F.	0	16806 S.F.	
21	NO TAKE			
22	1257 Ac.	0	4.654 Ac.	
22	9264 S.F.		REQD CHANNEL CHANGE AREA	
23	23623 S.F.	12930 S.F.	0	
24	3770 S.F.	36150 S.F.	0	
25	10312 S.F.		REQD CHANNEL CHANGE AREA	
26	251 Ac.	0	2115 Ac.	
27	SEE SHEET NO. 39			
28	SEE SHEET NO. 39			
29	SEE SHEET NO. 39			
30	SEE SHEET NO. 39			
20A	SEE SHEET NO. 32			
20B	SEE SHEET NO. 32			

* Temporary Easement for Construction not included in area of Take: 6879 S.F.

Parcel No.	Deed Book	Page
9	SHEET NO. 32	
10	1675	358
11	1675	362
12	1713	278
21	1703	20
22	1357	121
22	1386	399
22	1386	405
23	1594	537
24	1841	727
25	1601	47
26	781	60
27	SHEET NO. 39	
28	SHEET NO. 39	
29	SHEET NO. 39	
39	SHEET NO. 39	

Alignment	L.R. 1062	F.B. 48270
Alignment	Ramps	F.B. 48271
Topo	L.R. 1062	F.B. 48273
Topo	Ramps	F.B. 48276
Profile	L.R. 1062	F.B. 48278
Bench Levels		F.B. 48283
Reference Ties		F.B. 48284



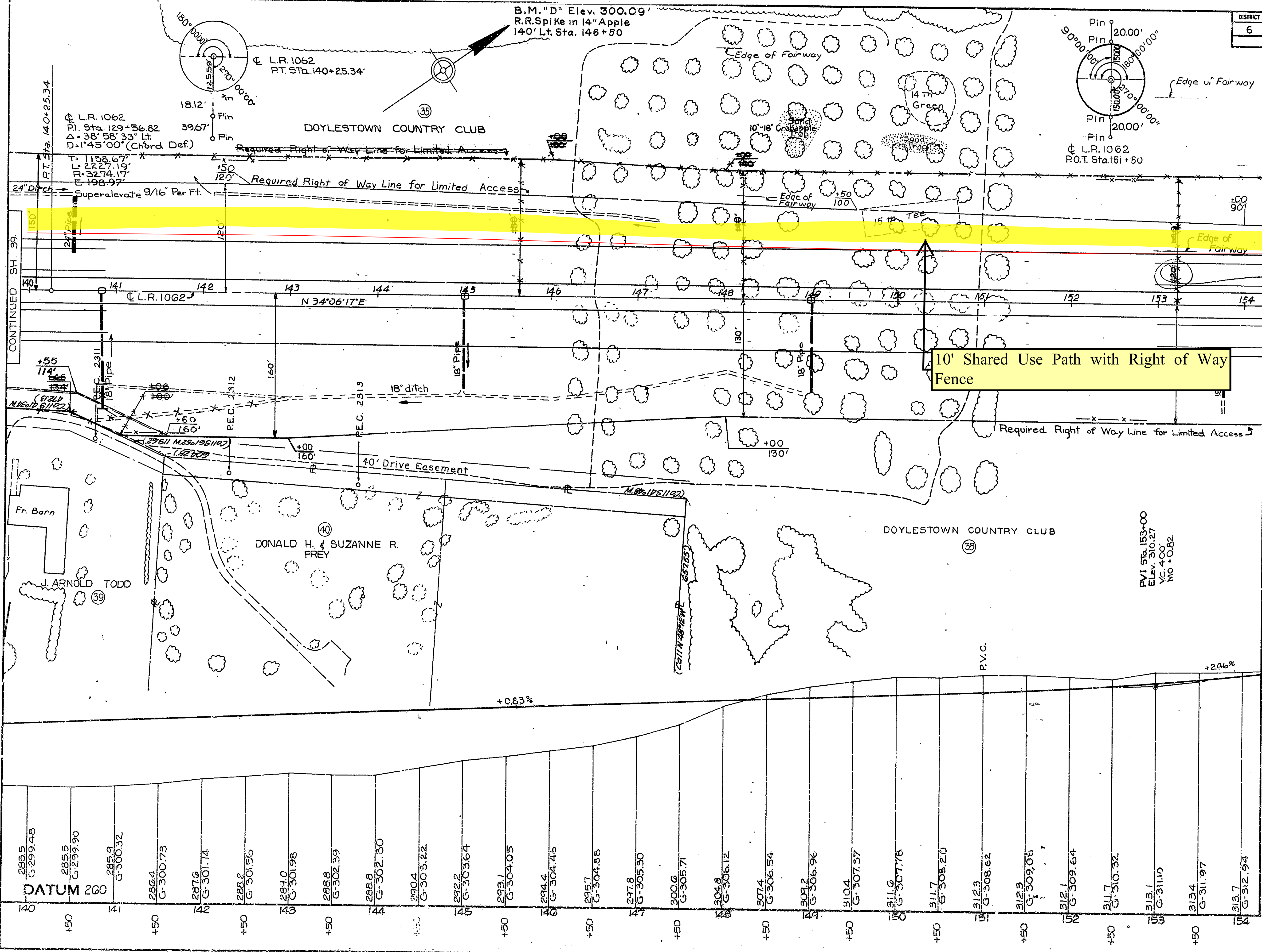
DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	1R/W	39 OF 52
DOYLESTOWN TOWNSHIP				

Parcel No.	Area Taken		Residue	
	R	W	Rt.	Lt.
26	SEE SHEET NO. 38			
27	.854 Ac.		0	.680 Ac.
27	330 S.F.		TEMP EASEMENT AREA	
28	3540 Ac.		.207 Ac.	.134 Ac.
29	5220 S.F.		REQD. CHANNEL CHANGE AREA	
29	2.053 Ac.		1.450 Ac.	0
29	2054 S.F.		REQD. CHANNEL CHANGE AREA	
30	503.456 Ac.		2.277 Ac.	10065 Ac.
31	SEE SHEET NO. 40			
32	.176 Ac.		0	.821 Ac.
33	SEE SHEET NO. 40			
35	14.069 Ac.		15.647 Ac.	97.508 Ac.
37	.268 Ac.		0	.866 Ac.
38	.031 Ac.		0	3.029 Ac.
39	NO TAKE			

Parcel No.	Deed Book	Page
26	SHEET NO. 38	
27	904	336
28	677	300
28	1150	210
29	1148	435
30	1182	454
31	SHEET NO. 40	
32	921	140
33	SHEET NO. 40	
35	867	317
35	1768	1077
37	1803	164
38	1591	99
39	1591	6

Alignment L.R. 1062 F.B. 48270
 Alignment L.R. 09049 F.B. 48271
 Topo L.R. 1062 F.B. 48273, 48274
 Topo L.R. 09049 F.B. 48275
 Bench Levels F.B. 48283
 Profiles L.R. 1062 F.B. 48272
 Profiles L.R. 09049 F.B. 48280
 Reference Ties F.B. 48284

DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	1R/W	41 OF 52
DOYLESTOWN TOWNSHIP				



Parcel No.	Area Taken		Residue	
	R W	Rt.	Rt.	Lt.
35	SEE SHEET NO. 39			
39	SEE SHEET NO. 39			
40	NO TAKE			

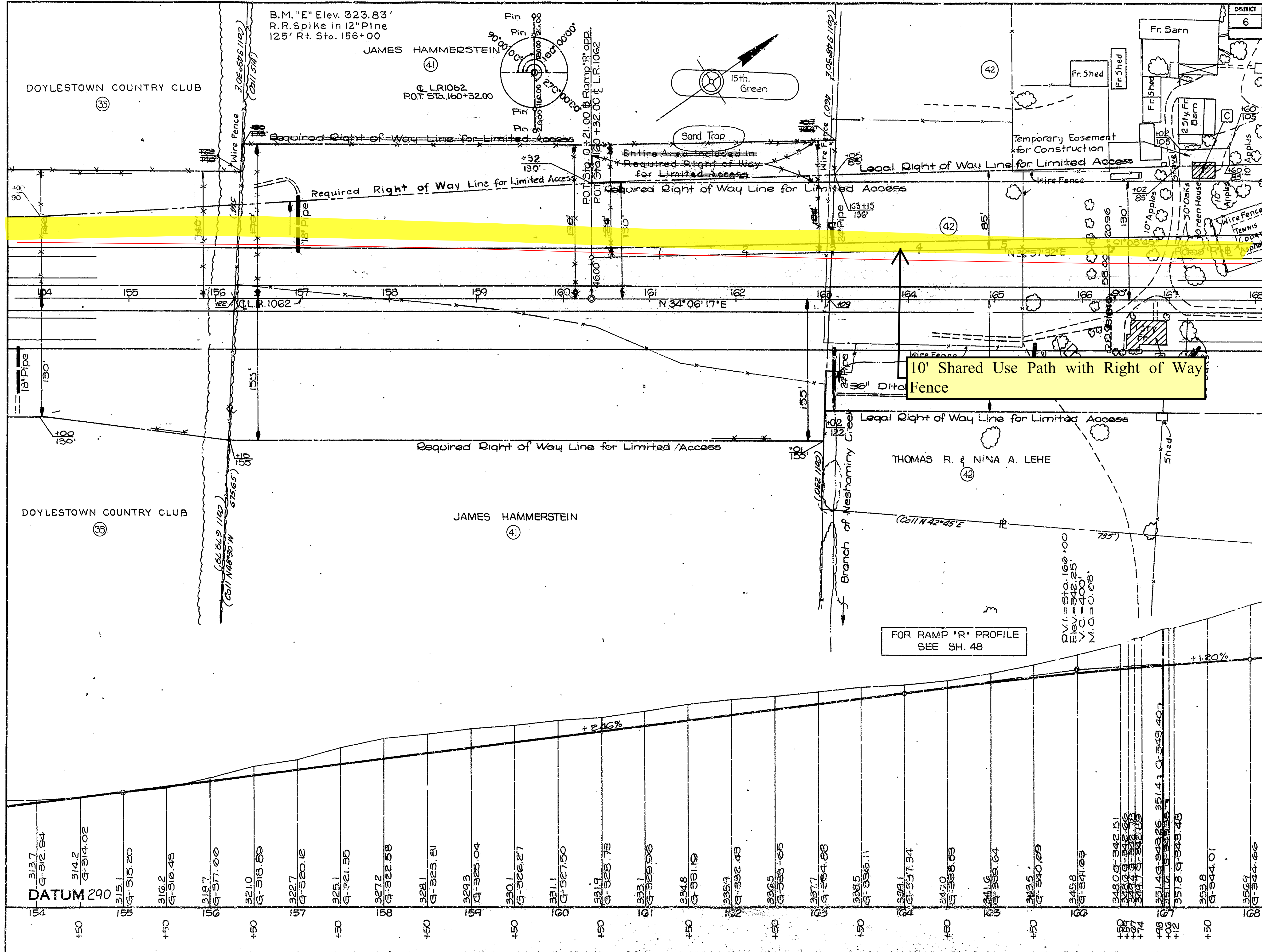
Parcel No.	Deed Book	Page
35	SHEET NO. 39	
39	SHEET NO. 39	
40	962	258

Alignment FB. 48270
 Topo FB. 48273
 Bench Levels FB. 48283
 Profile FB. 48278
 Reference Ties FB. 48284

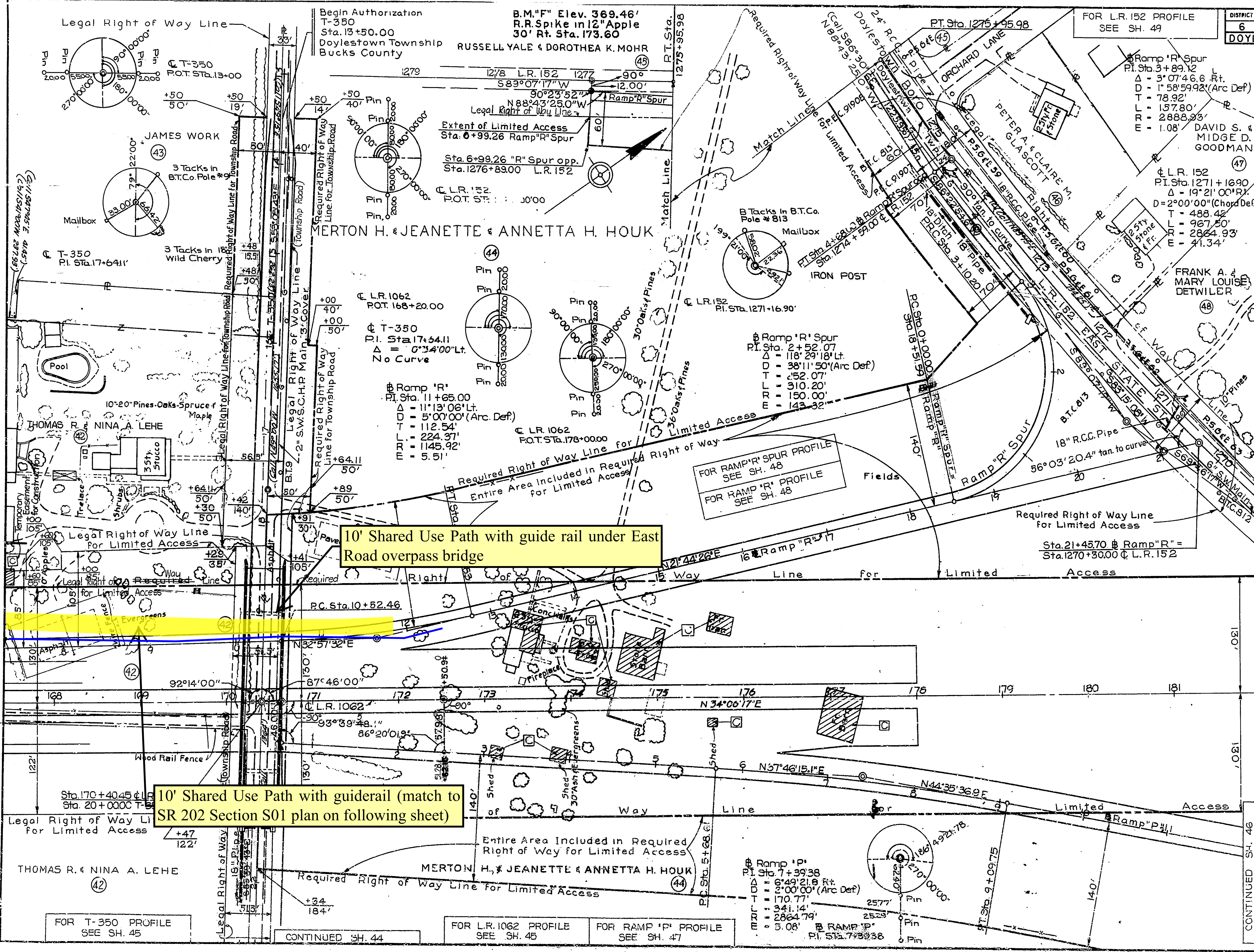
DISTRICT	COUNTY	L.R.	SECTION	SHEET
6	BUCKS	1062	IR/W	42 OF 52
DOYLESTOWN TOWNSHIP				

Parcel No.	Area Taken		Residue	
	R W	Rt.	Lt.	
35	SEE SHEET NO. 39			
41	5.72 Ac.	17.814 Ac	5.673 Ac.	
42	SEE SHEET NO. 43			

Parcel No.	Deed Book	Page
35	SEE SHEET NO. 39	
41	1785	536
42	SHEET NO. 43	



Alignment	L.R. 1062	F.B. 48270
Alignment	Ramp "R"	F.B. 48271
Topo	L.R. 1062	F.B. 48273
Topo	Ramp "R"	F.B. 48276
Bench Levels		F.B. 48233
Ref Ties		F.B. 48284
Profile	L.R. 1062	F.B. 48278



10' Shared Use Path with guide rail under East Road overpass bridge

10' Shared Use Path with guiderail (match to SR 202 Section S01 plan on following sheet)

FOR L.R. 152 PROFILE
SEE SH. 49

FOR RAMP 'R' SPUR PROFILE
SEE SH. 48

FOR T-350 PROFILE
SEE SH. 45

FOR L.R. 1062 PROFILE
SEE SH. 45

FOR RAMP 'R' PROFILE
SEE SH. 47

FOR RAMP 'R' PROFILE
SEE SH. 48

DISTRICT	CONTRACT	L.P.	T.C.	SHEET
6	BUCKS	1762	I-R/W	43 OF 52

Parcel No.	Area Taken		Residue	
	R W	Rt.	Rt.	Lt.
42	NO TAKE			
43	.145 Ac.	10.357 Ac.		0
44	19.540 Ac.	39.845 Ac.		12.557 Ac.
45	NO TAKE			
46	NO TAKE			
47	NO TAKE			
48	NO TAKE			
42	.040 AC.		TEMP. EASEMENT AREA	

Parcel No.	Deed Book	Page
42	1611	406
42	1611	409
43	1767	882
43	1771	298
43	1799	299
44	994	44
45	1768	365
46	1734	553
47	1879	880
48	1296	527

Alignment	L.R. 1062	F.B. 48270
Alignment	Ramps	F.B. 48271
Topo	L.R. 1062	F.B. 48274
Topo	Ramps	F.B. 48276
Bench Levels		F.B. 48283
Reference Ties		F.B. 48284
T-350		F.B. 48287
L.R. 152		F.B. 48287

CONTINUED SH. 46

I-102A
TYPE M INLET
STANDARD BOX
STA 549+27.00
OFFSET 65.50' LT
GRATE=344.95
INV OUT=341.73

I-102B
TYPE M INLET
STANDARD BOX
WITH STEPS
STA 549+33.00
OFFSET 22.25' RT
GRATE=343.75
INV IN=339.21 (I-102A)
INV IN=339.21 (I-102C)
INV IN=338.24 (I-103)
INV OUT=338.21

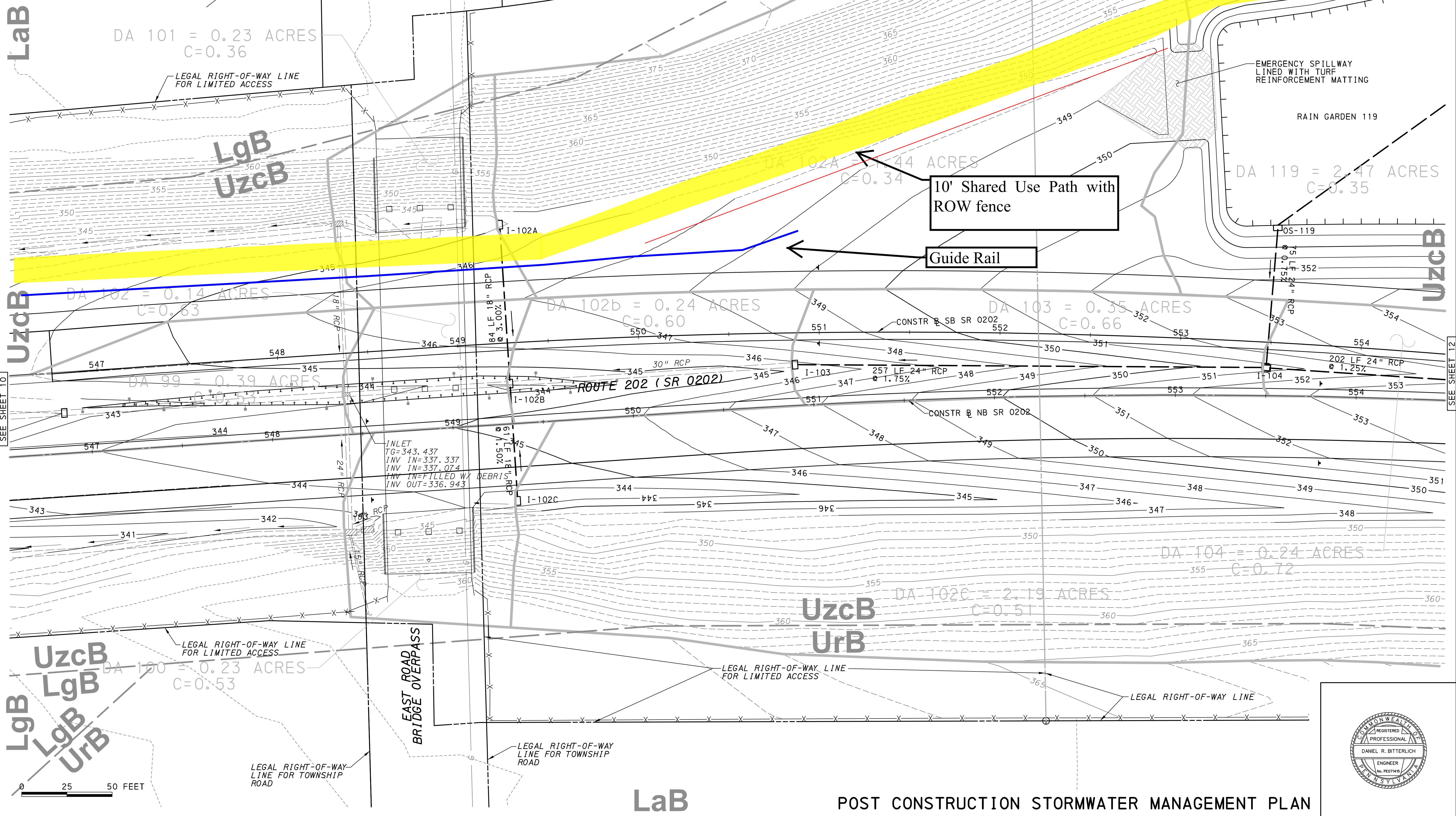
I-102C
TYPE M INLET
STANDARD BOX
STA 549+33.00
OFFSET 42.75' RT
GRATE=343.36
INV OUT=340.13

I-103
TYPE M INLET
STANDARD BOX
STA 550+91.00
OFFSET 21.50' LT
GRATE=345.12
INV IN=340.44
INV OUT=340.27

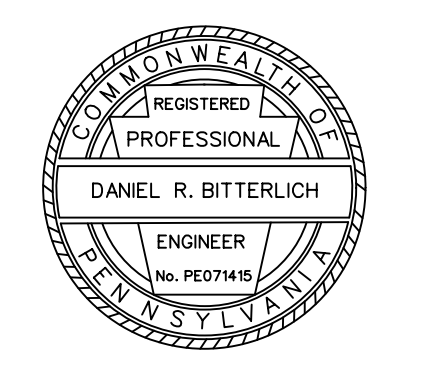
I-104
TYPE M INLET
TYPE 4 BOX
WITH STEPS
STA 553+50.00
OFFSET 15.50' LT
GRATE=350.60
INV IN=345.23 (OS-119)
INV IN=346.66 (I-105)
INV OUT=344.94

OS-119
OUTLET STRUCTURE
STANDARD BOX
STA 553+50.00
OFFSET 63.50' LT
ELEV=348.50
INV OUT=345.79

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	0202	S01	11 of 24
DOYLESTOWN & BUCKINGHAM TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



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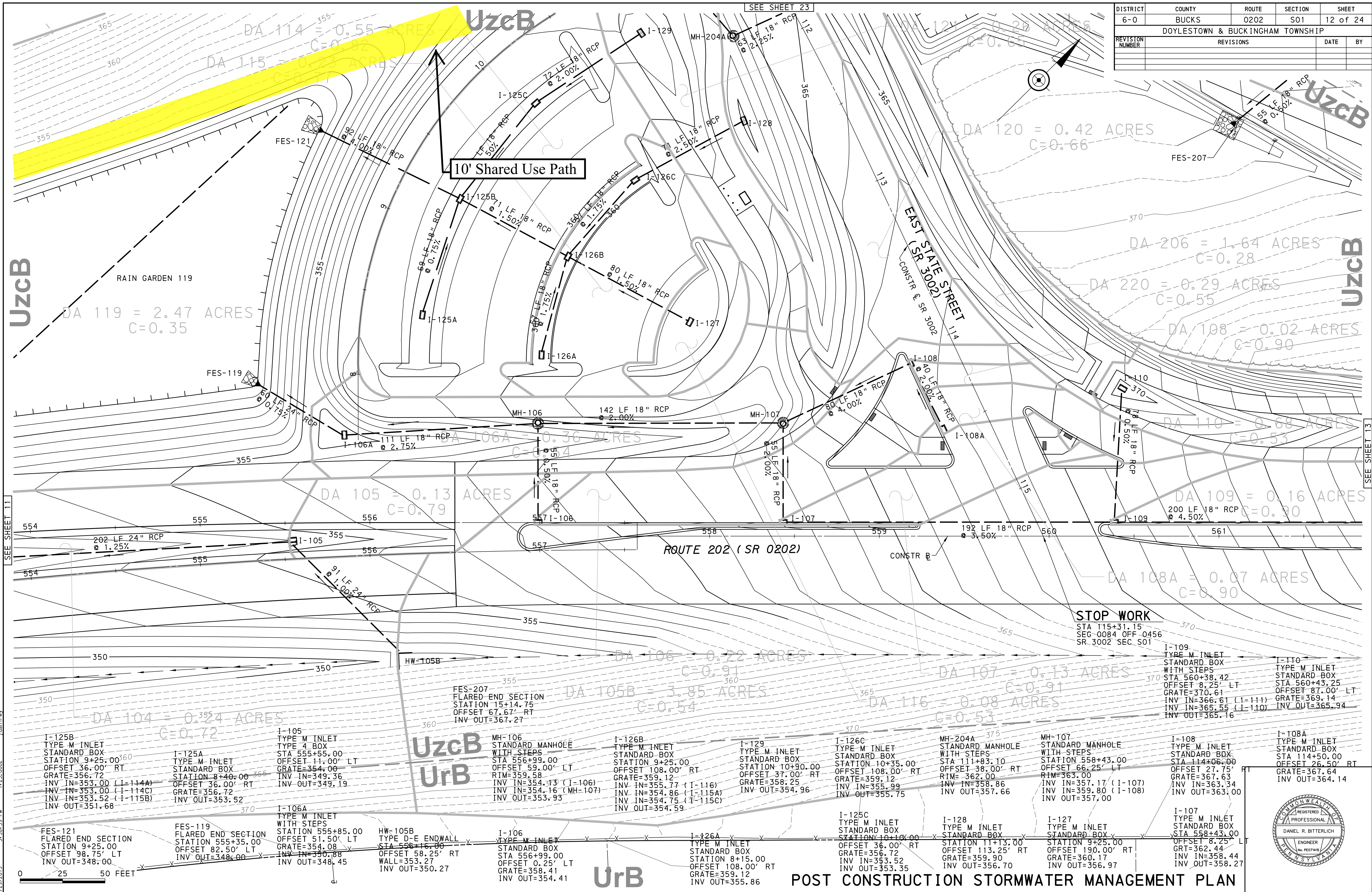


LaB POST CONSTRUCTION STORMWATER MANAGEMENT PLAN

SEE SHEET 10

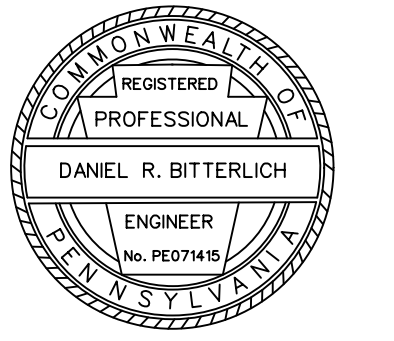
SEE SHEET 12

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	0202	S01	12 of 24
DOYLESTOWN & BUCKINGHAM TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



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I-125B TYPE M INLET STANDARD BOX STATION 9+25.00 OFFSET 36.00' RT GRATE=356.72 INV IN=353.00 (I-114A) INV IN=353.00 (I-114C) INV IN=353.52 (I-115B) INV OUT=351.68	I-125A TYPE M INLET STANDARD BOX STATION 8+40.00 OFFSET 36.00' RT GRATE=356.72 INV OUT=353.52	I-105 TYPE M INLET TYPE 4 BOX STA 555+55.00 OFFSET 11.00' LT GRATE=354.00 INV IN=349.36 INV OUT=349.19	MH-106 STANDARD MANHOLE WITH STEPS STA 556+99.00 OFFSET 59.00' LT RIM=359.58 INV IN=354.13 (I-106) INV IN=354.16 (MH-107) INV OUT=353.93	I-126B TYPE M INLET STANDARD BOX STATION 9+25.00 OFFSET 108.00' RT GRATE=359.12 INV IN=355.77 (I-116) INV IN=354.86 (I-115A) INV IN=354.75 (I-115C) INV OUT=354.59	I-129 TYPE M INLET STANDARD BOX STATION 10+90.00 OFFSET 37.00' RT GRATE=358.25 INV OUT=354.96	I-126C TYPE M INLET STANDARD BOX STATION 10+35.00 OFFSET 108.00' RT GRATE=359.12 INV IN=355.99 INV OUT=355.75	MH-204A STANDARD MANHOLE WITH STEPS STA 111+83.10 OFFSET 38.00' RT RIM=362.00 INV IN=358.86 INV OUT=357.66	MH-107 STANDARD MANHOLE WITH STEPS STATION 558+43.00 OFFSET 66.25' LT RIM=363.00 INV IN=357.17 (I-107) INV IN=359.80 (I-108) INV OUT=357.00	I-108 TYPE M INLET STANDARD BOX STA 114+06.00 OFFSET 27.75' RT GRATE=367.63 INV IN=363.34 INV OUT=363.00	I-108A TYPE M INLET STANDARD BOX STA 114+50.00 OFFSET 26.50' RT GRATE=367.64 INV OUT=364.14
FES-121 FLARED END SECTION STATION 9+25.00 OFFSET 98.75' LT INV OUT=348.00	FES-119 FLARED END SECTION STATION 555+35.00 OFFSET 82.50' LT INV OUT=348.00	I-106A TYPE M INLET WITH STEPS STATION 555+85.00 OFFSET 51.50' LT GRATE=354.08 INV IN=350.88 INV OUT=348.45	HW-105B TYPE D-E ENDWALL STA 556+16.00 OFFSET 58.25' RT WALL=353.27 INV OUT=350.27	I-106 TYPE M INLET STANDARD BOX STA 556+99.00 OFFSET 0.25' LT GRATE=358.41 INV OUT=354.41	I-126A TYPE M INLET STANDARD BOX STATION 8+15.00 OFFSET 108.00' RT GRATE=359.12 INV OUT=355.86	I-125C TYPE M INLET STANDARD BOX STATION 10+100.00 OFFSET 36.00' RT GRATE=356.72 INV IN=353.35 INV OUT=353.35	I-128 TYPE M INLET STANDARD BOX STATION 11+13.00 OFFSET 113.25' RT GRATE=359.90 INV OUT=356.70	I-127 TYPE M INLET STANDARD BOX STATION 9+25.00 OFFSET 190.00' RT GRATE=360.17 INV OUT=356.97	I-107 TYPE M INLET STANDARD BOX STA 558+43.00 OFFSET 8.25' LT GRATE=362.44 INV IN=358.44 INV OUT=358.27	



POST CONSTRUCTION STORMWATER MANAGEMENT PLAN

FES-224
FLARED END SECTION
STATION 105+45.00
OFFSET 47.05' RT
INV OUT=349.90

FES-225
FLARED END SECTION
STATION 106+35.00
OFFSET 50.25' RT
INV IN=351.25

I-203
TYPE M INLET
STANDARD BOX
STATION 108+90.00
OFFSET 46.00' LT
GRATE=356.90
INV OUT=353.69

HW-205
TYPE D-W ENDWALL
STATION 111+86.50
OFFSET 56.25' LT
WALL=363.39
INV OUT=360.68

I-202
TYPE M INLET
STANDARD BOX
STATION 106+94.00
OFFSET 37.00' LT
GRATE=355.37
INV OUT=352.17

MH-202
STANDARD MANHOLE
STA 106+94.00
OFFSET 21.00' LT
RIM= 355.85
INV IN=351.52 (EX-203)
INV IN=351.96 (I-202)
INV OUT=351.46

I-220
TYPE M INLET
STANDARD BOX
STATION 111+88.83
OFFSET 26.67' LT
GRATE=363.75
INV IN=360.46
INV OUT=360.28

I-129
TYPE M INLET
STANDARD BOX
STATION 10+90.00
OFFSET 37.00' RT
GRATE=358.25
INV OUT=354.96

MH-203
STANDARD MANHOLE
WITH STEPS
STA 108+91.18
OFFSET 35.20' LT
RIM= 360.55
INV IN=353.54 (MH-204)
INV IN=353.54 (I-203)
INV OUT=353.48

I-204
TYPE M INLET
STANDARD BOX
STATION 110+05.00
OFFSET 71.00' LT
GRATE=359.04
INV OUT=355.84

I-128
TYPE M INLET
STANDARD BOX
STATION 11+13.00
OFFSET 113.25' RT
GRATE=359.90
INV OUT=356.70

MH-204A
STANDARD MANHOLE
WITH STEPS
STA 111+83.10
OFFSET 38.00' RT
RIM= 362.00
INV IN=358.86
INV OUT=357.66

MH-204
STANDARD MANHOLE
STA 110+07.25
OFFSET 60.75' LT
RIM= 360.00
INV IN=355.91 (JUNCTION)
INV IN=355.80 (I-204)
INV OUT=355.80

FES-226
FLARED END SECTION
STATION 109+78.00
OFFSET 60.50' RT
INV OUT=355.69

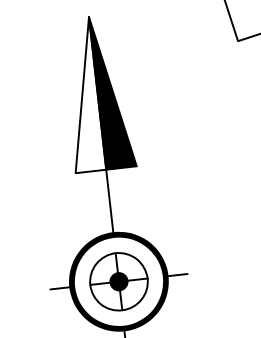
UrB

I-125C
TYPE M INLET
STANDARD BOX
STATION 10+10.00
OFFSET 36.00' RT
GRATE=356.72
INV IN=353.52
INV OUT=353.35

I-126C
TYPE M INLET
STANDARD BOX
STATION 10+35.00
OFFSET 108.00' RT
GRATE=359.12
INV IN=355.99
INV OUT=355.75

I-125B
TYPE M INLET
STANDARD BOX
STATION 9+25.00
OFFSET 36.00' RT
GRATE=356.72
INV IN=353.00 (I-114A)
INV IN=353.00 (I-114C)
INV IN=353.52 (I-115B)
INV OUT=351.68

DISTRICT	COUNTY	ROUTE	SECTION	SHEET
6-0	BUCKS	0202	S01	23 of 24
DOYLESTOWN & BUCKINGHAM TOWNSHIP				
REVISION NUMBER	REVISIONS	DATE	BY	



UrB

SEE SHEET 22

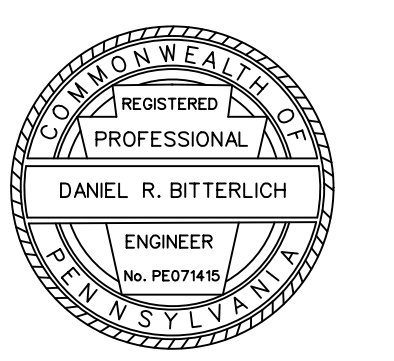
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UzCB

POST CONSTRUCTION STORMWATER MANAGEMENT PLAN



DA 201 = 0.51 ACRES
C=0.56

INLET
TG 353.46
INV IN 349.81
INV OUT 349.81

ORCHARD LANE

DA 202 = 0.40 ACRES
C=0.39

UrB
UzCB

LEGAL RIGHT-OF-WAY LINE

DA 204B = 0.28 ACRES
C=0.30

DA 203 = 0.91 ACRES
C=0.43

DA 208 = 14.86 ACRES
C=0.52, Tc=20 MIN

DA 206 = 1.64 ACRES
C=0.33

UrB

DA 117 = 0.28 ACRES
C=0.58

DA 204A = 0.28 ACRES
C=0.52

DA 120 = 0.42 ACRES
C=0.66

DA 121 = 0.26 ACRES
C=0.65

DA 225 = 1.39 ACRES
C=0.48

DA 200 = 0.14 ACRES
C=0.13

DA 114 = 0.55 ACRES
C=0.82

DA 115 = 0.23 ACRES
C=0.77

DA 101 = 2.47 ACRES
C=0.74

10' Shared Use Path

SEE SHEET 24

SEE SHEET 12

Virtual Public Meeting Room Information



Doylestown Bike and Hike Committee Presents:
202 Trail Feasibility Study – (Central Park to Route 313)
Virtual Public Meeting



As you may know, Doylestown Township/Borough have been working hard to bring you a robust network of walking and bicycle trails for over 25 years. Recently, a \$30k grant was awarded through the Delaware Valley Regional Planning Commission’s trails program for a feasibility study centered along Route 202 between Central Park and Route 313. This 2.8-mile-long project will help connect the 30+ mile Doylestown Community Bike and Hike network to Buckingham Township and extend the very popular 202 Parkway Trail. We invite you to attend a virtual public meeting to share your feedback on the progress of this project to date.

In a 3-D virtual setting, you will be able to:

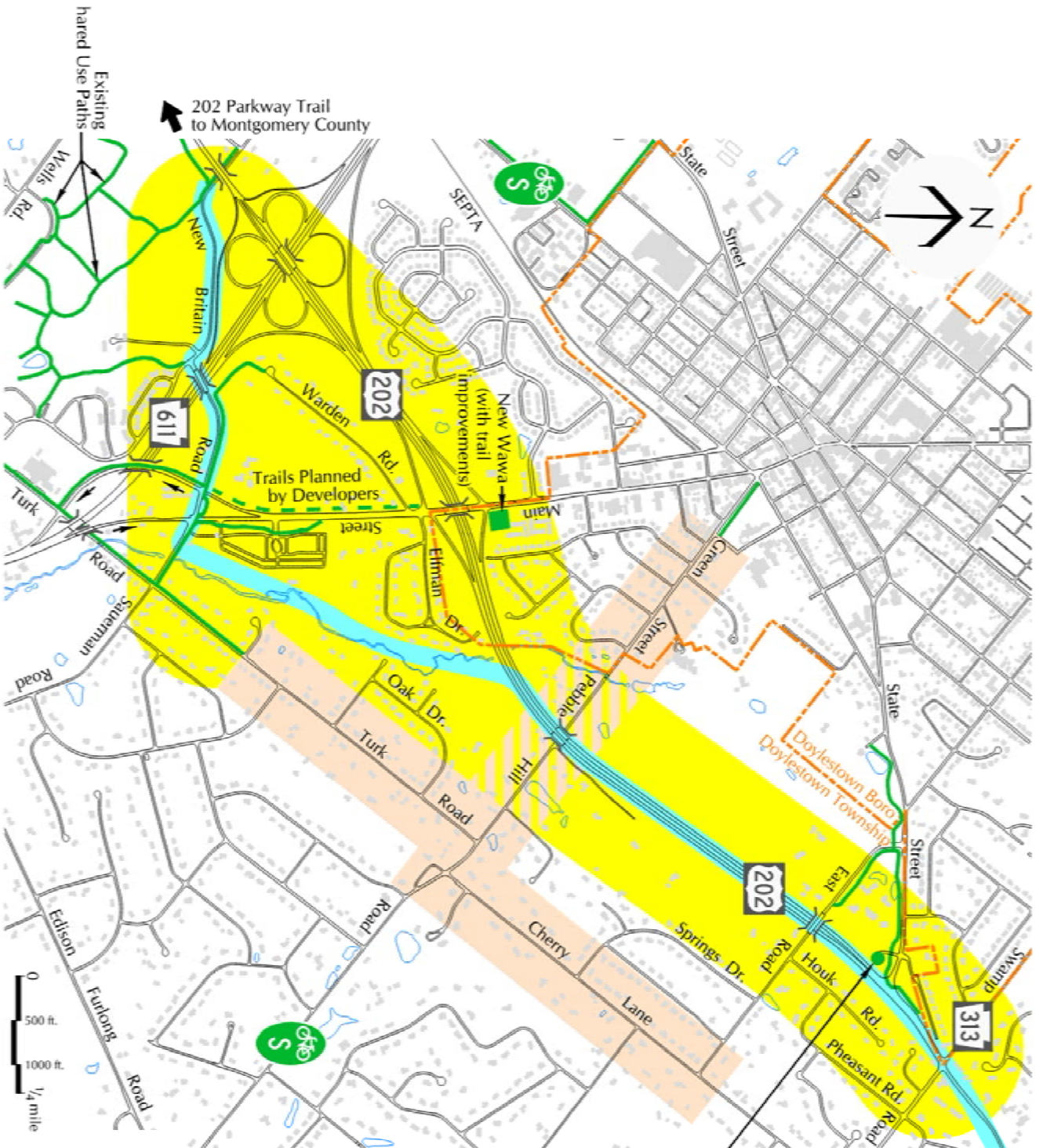
- Review project information
- Explore alternative trail alignments
- Learn about anticipated opportunities and challenges of this project
- Share feedback to help guide this project towards a successful completion.

The virtual public meeting room will be hosted for a two-week time period on Doylestown Township and Borough’s websites (Links Below) and we hope to receive your much-needed feedback to help progress this important link in the Bike and Hike network.

Date Range: Sunday, February 7th to Sunday, February 21st

Location: Virtual Meeting Room Link: <https://202trail.z13.web.core.windows.net/>
 Also found on Doylestown Borough Website: <https://www.doylestownborough.net/>
 And Doylestown Township Website: <https://doylestownpa.org/>

Contacts: Steve DiSciullo - (215) 442-5329 - steven.disciullo@mbakerintl.com
 Stephanie Mason - (215) 348-9915 - sjmason@doylestownpa.org
 John Davis - (215) 345-4140 - jdavis@doylestownborough.net



The Circuit Trails Planned to New Hope

Existing Trailhead Parking Lot

Legend

- The Circuit Trails - Planned Trail
- Proposed Study Corridor
- Pebble Hill Road Ongoing Study
- Existing Shared Use Path
- Municipal Boundary

Map of Proposed Study Corridor

Doylestown Township
 Doylestown Borough
 Bucks County, Pennsylvania

F.A.Q.



202 Trail Feasibility Study - (Central Park to Route 313) Frequently Asked Questions

- **Question: Why do we want to build a trail from Central Park to Route 313?**
 - Answer: As you may know, Doylestown has a network of ~30 miles of completed trails that are extremely popular in the local walking and biking community. This project will investigate an extension of the 202-parkway trail, while increasing the network's footprint to reach additional township and borough residents. Regionally speaking, this section of trail is part of the long-term plan to extend a trail from Doylestown to New Hope along the Route 202 corridor.

- **Question: What's included in this study?**
 - Answer: The study will investigate two alternative alignments from Central Park (south) to route 313 (North) generally centered along Route 202. Through this investigation the planning team will investigate right-of-way impacts (if any), identify opportunities and constraints, and determine if any environmental or cultural impacts are present. After this public engagement process, the planning team will weigh the alternatives to develop a preferred route with a cost estimate to the project's stakeholders. All this information will be organized into a concise study report for future reference.

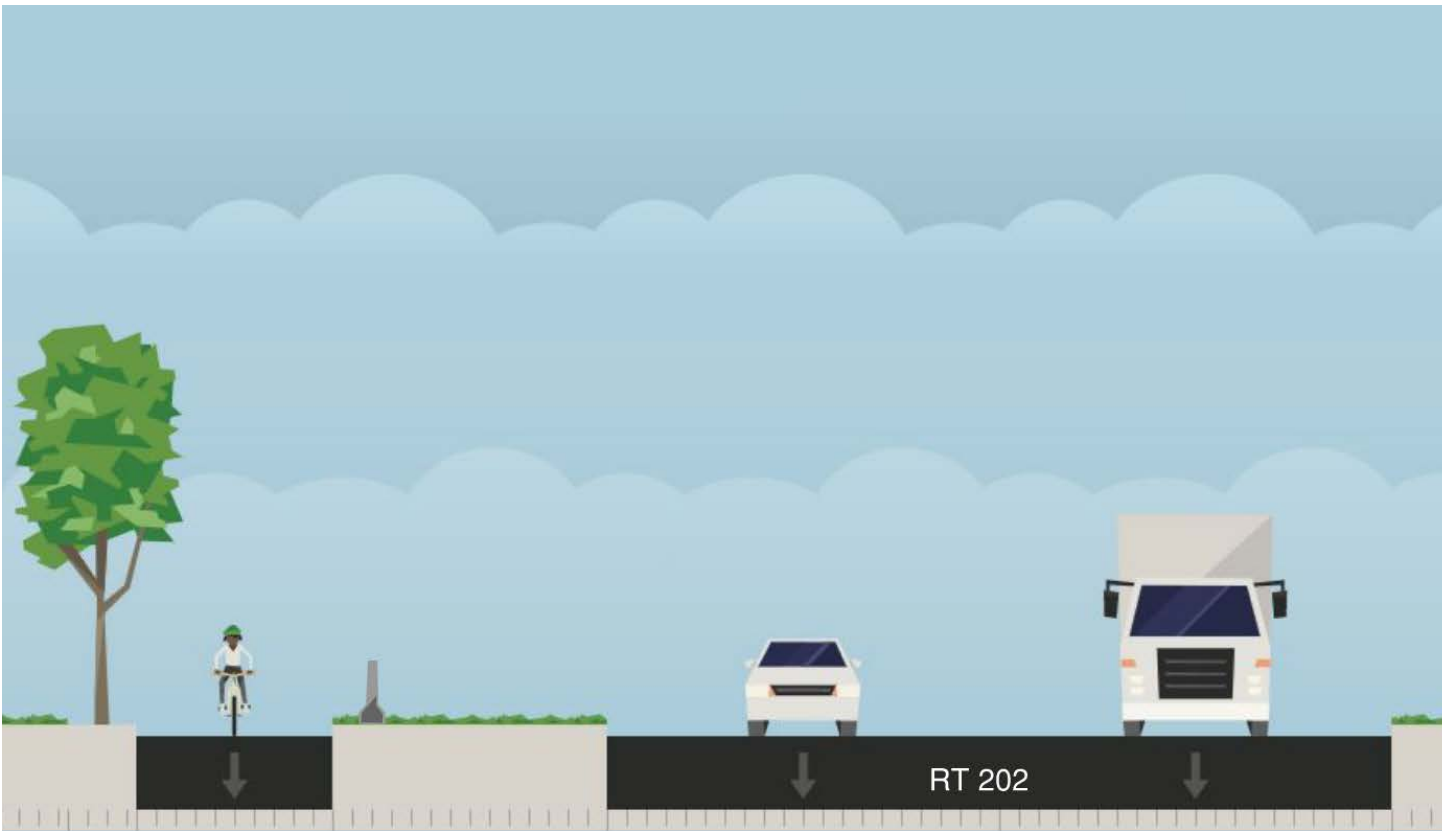
- **Question: When will the trail construction take place?**
 - Answer: We are still years away from completing the construction of this trail. This project is currently in the planning phase where teams of engineers, planners, **and the public** will assess the trail for feasibility. If the planning phase is successful, we can then move on to engineering design. Trail construction will occur after all design approvals are completed. The anticipated completion date for this feasibility study is April 2021.

- **Question: How is the planning, design and construction of these trails funded?**
 - Answer: This trail study was funded with a \$30k grant from Delaware Valley Regional Planning Commission (DVRPC) Regional Trails Grant Program (link below). Historically, Doylestown has funded most of their trail planning, design and construction using a combination of grant funding from local, state, and federal sources. Doylestown Township/Borough typically match these grants with a ~15%-50% fund contribution. These sources include:
 - Delaware Valley Regional Planning Commission (DVRPC) Regional Trails Grant Program
 - Department of Conservation and Natural Resources (DCNR) Trail Grants
 - PennDOT's Transportation Alternatives Set-Aside program
 - Pennsylvania Department of Community and Economic Development (DCED) Greenways, Trails and Recreation Program

- **Question: Who is responsible for maintaining the trail such as winter snow removal?**
 - Answer: Doylestown Township, and Doylestown Borough will accept full maintenance responsibility for the new trail including winter snow removal and structural repair.

- **Question: What if I still have questions about this trail?**
 - Answer: Additional information regarding this trail and similar trails can be found on the following township websites:
 - Doylestown Township - Biking (<https://doylestownpa.org/twp-information/biking/>)
 - Doylestown Township Parks and Rec – Trails (<https://doylestownpa.org/parks-rec-home/trails/>)You may also contact Doylestown Township's Manager, Stephanie Mason or Doylestown Borough's Manager, John Davis with additional questions.

- **Question: Are any easements anticipated**
 - Answer: The entire proposed trail is within the PennDOT, and Township owned Right-of-way. Easements are not anticipated at this point.

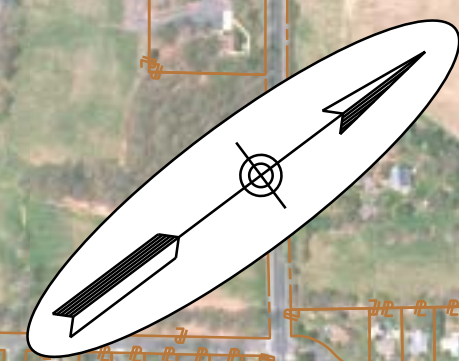



Project Information

- Project Title: 202 Trail Feasibility Study
- Starting Point: Central Park
- Ending Point: Poole's Corner Park and Ride
- Trail Length: ~3 miles
- Trail Width: 10 feet
- Trail Type: Asphalt
- Buffer: Trees, Guiderail, Plants, Shrubbery, Large Grass areas and/or Fencing.

10-FEBR-2021

EA:\166816_Turkey_Trail_Study\PIA\01\Map\Borochs\Borochs_VR-313A\B1313_Overall_Trail_Plan.dwg







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 **DOYLESTOWN TOWNSHIP**

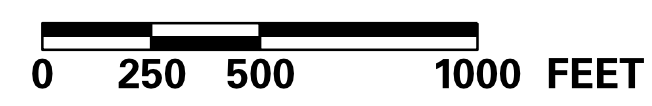
PREPARED BY:
Michael Baker INTERNATIONAL
 500 OFFICE CENTER DRIVE SUITE 210
 FORT WASHINGTON, PA 19034

SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
 TRAIL FEASIBILITY STUDY
 DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

-  PROPOSED SHARED USE PATH/TRAIL - RED ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - YELLOW ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - BLUE ROUTE
-  PROPOSED SHARED USE PATH/TRAIL - BLACK ROUTE

-  PLANNED TRAILS
-  EXISTING TRAIL



	OPPORTUNITIES ✓	CONSTRAINTS X
Red Route	<ul style="list-style-type: none"> • Potential Access points for Residents of Doylestown Hunt Estates • Connection to Pebble Hill Road • Connection to Lower St Rd Trail • Connection to existing Bike/Walk routes to the borough at Wawa • Potential Access Points for Borough Residents along near Meadow Ln, Scott Rd, etc. 	<ul style="list-style-type: none"> • A Mid-Block crossing must be added along Lower State Road • SEPTA Rail Crossing is required • Large Bicycle/Pedestrian bridge required to cross Pebble Hill Road • Less direct route compared to black and yellow by the 611 cloverleaf. • Errant golf balls could present a hazard if no fence is added along Doylestown Country Club
Yellow Route	<ul style="list-style-type: none"> • Potential Access points for Residents along Warden Road • Connection to existing trails on New Britain Road and Central Park • Potential Access points for Residents near Elfman Drive 	<ul style="list-style-type: none"> • Large Bicycle/Pedestrian bridge required to cross Pebble Hill Road • Modification of New Britain Rd Bridge is anticipated • Intersection improvements anticipated near Poole’s corner
Blue Route	<ul style="list-style-type: none"> • Potential Access points for Doylestown Hunt Estates Residents • Connection to existing Bike/Walk routes to the borough at Wawa • Potential Access points for Residents near Elfman Drive • Connection to Lower St Rd Trail 	<ul style="list-style-type: none"> • A Mid-Block crossing must be added along Lower State Road • SEPTA Rail Crossing is required • Large Bicycle/Pedestrian bridge required to cross Pebble Hill Road • Less direct route compared to black and yellow by the 611 cloverleaf. • Intersection improvements anticipated near Poole’s corner
Black Route	<ul style="list-style-type: none"> • Connection to Pebble Hill Road • Scenic Golf Course Views • Connection to existing Bike/Walk routes to the borough at Wawa 	<ul style="list-style-type: none"> • Errant golf balls could present a hazard if no fence is added along Doylestown Country Club • Large Bicycle/Pedestrian bridge required to cross Pebble Hill Road • Modification of New Britain Rd Bridge is anticipated



LOOKING EASTBOUND ALONG LOWER STATE RD



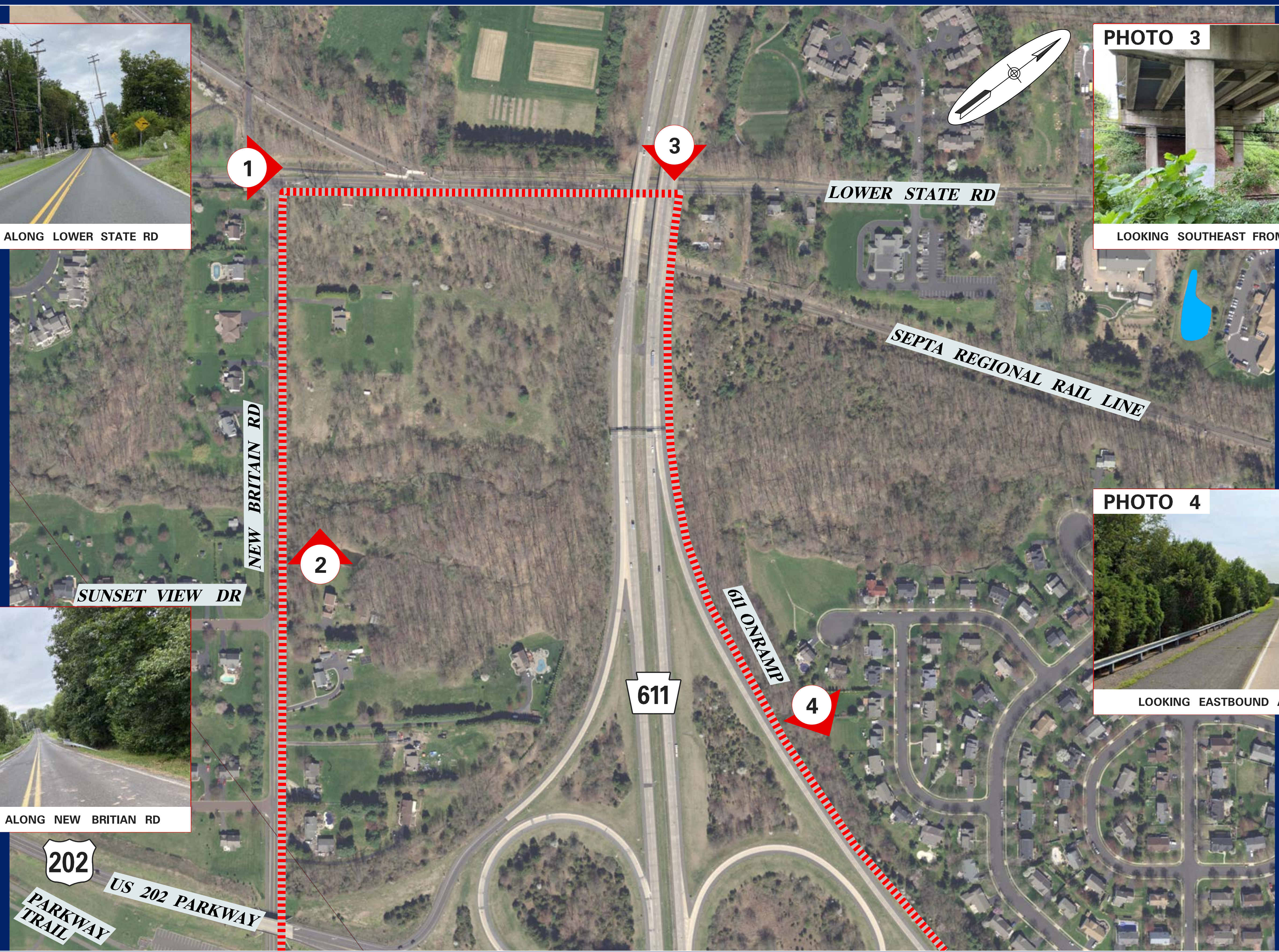
LOOKING SOUTHEAST FROM LOWER STATE ROAD



LOOKING WESTBOUND ALONG NEW BRITIAN RD



LOOKING EASTBOUND ALONG 611 ONRAMP



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DOYLESTOWN TOWNSHIP

PREPARED BY:




Michael Baker

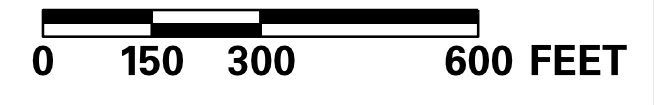
INTERNATIONAL
500 OFFICE CENTER DRIVE, SUITE 210,
FORT WASHINGTON, PA 19034-3234
Phone: (215) 444-0888 - MBAKERINTL.COM

*SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
TRAIL FEASIBILITY STUDY*

DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

-  PHOTO LOCATION
-  PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 1A & 1B
-  PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 2A & 2B



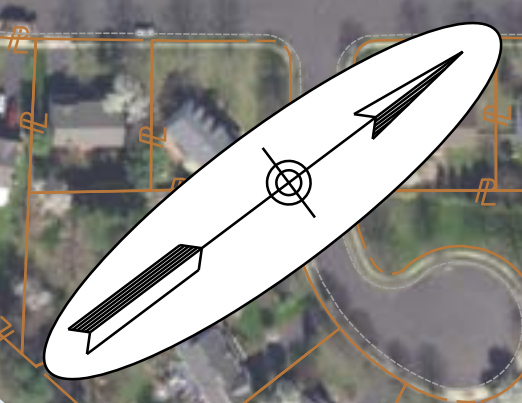
PARKWAY TRAIL

5

US 202 PARKWAY

202

611 ONRAMP



6

NEW BRITAIN RD

WARDEN RD

8

9

7

611

NEW BRITAIN RD

PHOTO 5



LOOKING EASTBOUND ALONG NEW BRITIAN ROAD

PHOTO 6



LOOKING EASTBOUND ALONG NEW BRITIAN ROAD

PHOTO 8



LOOKING NORTHWEST ALONG EXISTING TRAIL BETWEEN NEW BRITIAN ROAD AND WARDEN ROAD

PHOTO 7



LOOKING NORTHEAST ACROSS NEW BRITIAN RD BRIDGE

PHOTO 9



LOOKING NORTHWEST ALONG EXISTING TRAIL BETWEEN NEW BRITIAN ROAD AND WARDEN ROAD

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DOYLESTOWN TOWNSHIP

PREPARED BY:

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SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
TRAIL FEASIBILITY STUDY

DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

- PHOTO LOCATION
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 1A & 1B
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 2A & 2B

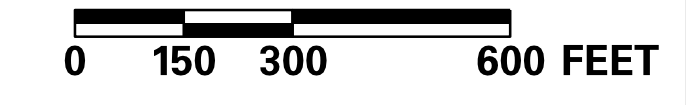


PHOTO 10



LOOKING NORTHEAST ALONG 202 ON RAMP 1

PHOTO 12



INTERSECTION OF 202 ON RAMP 1 AND EASTON ROAD/MAIN STREET

PHOTO 11

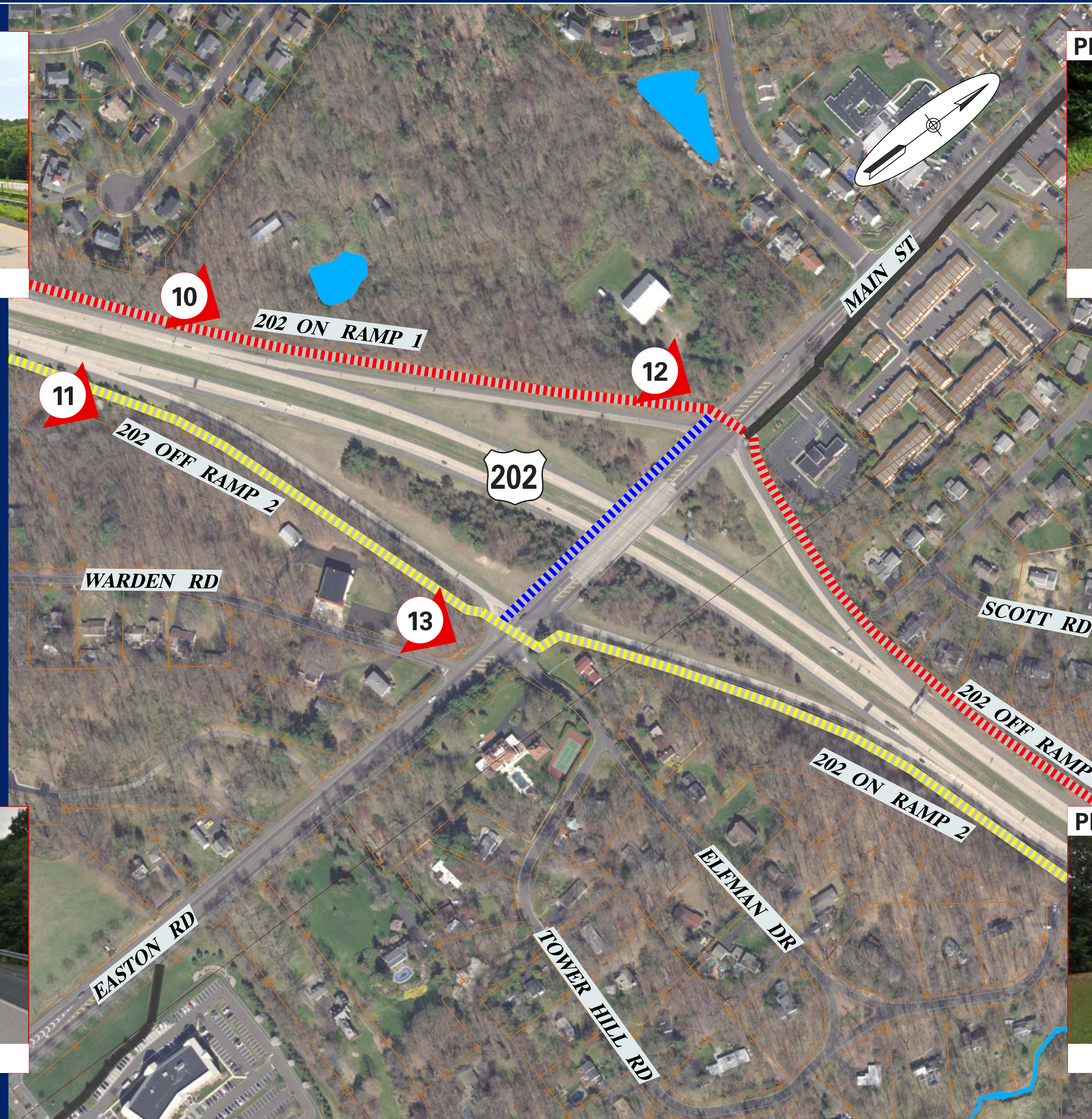


LOOKING NORTHEAST ALONG 202 OFF RAMP 2

PHOTO 13



INTERSECTION OF 202 OFF RAMP 2 AND EASTON ROAD/MAIN STREET



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PREPARED BY:

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*SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
TRAIL FEASIBILITY STUDY*

DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

- PHOTO LOCATION
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 1A & 1B
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 2A & 2B
- PROPOSED TRAIL CONNECTOR

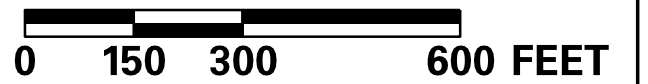


PHOTO 14



202 SOUTH BOUND (LOOKING NORTH)

PHOTO 15



LOOKING FROM PEBBLE HILL ROAD/GREEN STREET NORTH

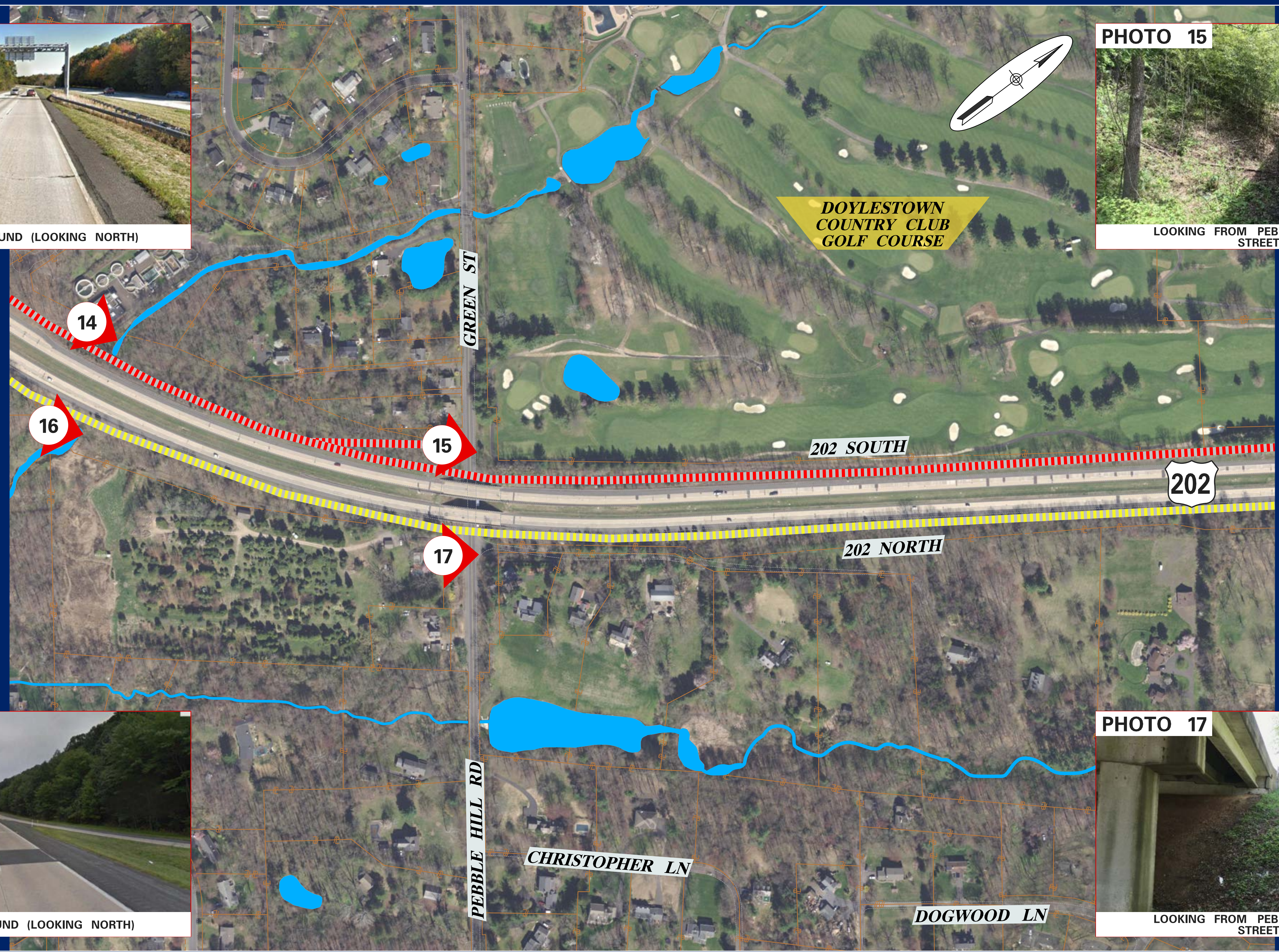


PHOTO 16



202 NORTHBOUND (LOOKING NORTH)

PHOTO 17



LOOKING FROM PEBBLE HILL ROAD/GREEN STREET NORTH

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PREPARED FOR:



DOYLESTOWN TOWNSHIP

PREPARED BY:

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*SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
TRAIL FEASIBILITY STUDY*

DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

- PHOTO LOCATION
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 1A & 1B
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 2A & 2B

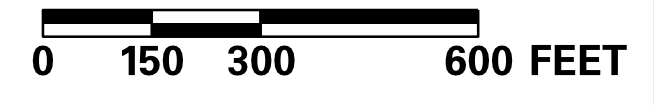


PHOTO 18



LOOKING NORTHBOUND ALONG 202 SOUTH

PHOTO 20



LOOKING NORTHBOUND ALONG 202 SOUTH TOWARDS EAST ROAD BRIDGE

PHOTO 21



LOOKING NORTHBOUND ALONG 202 NORTH TOWARDS EAST ROAD BRIDGE

PHOTO 22

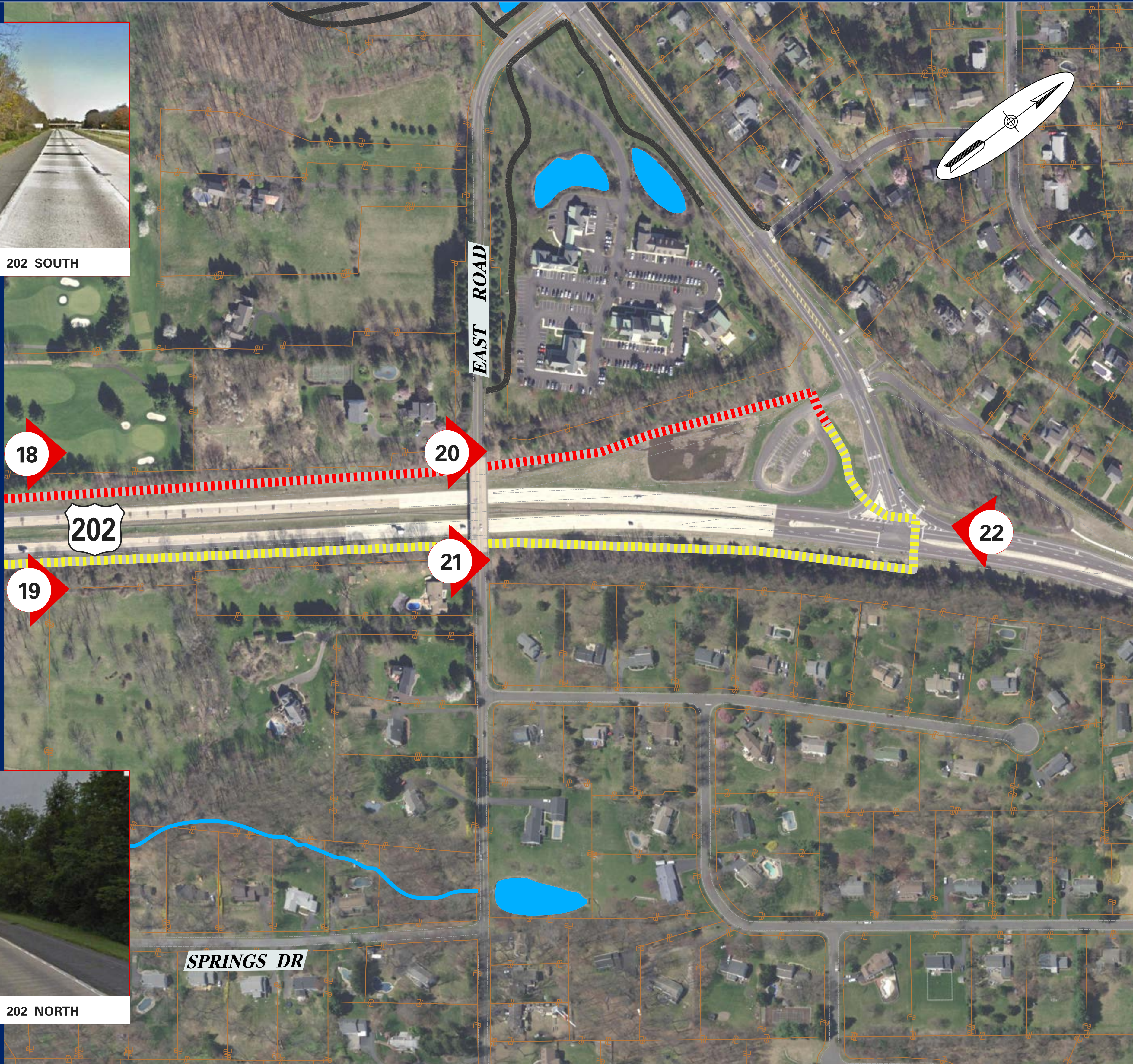


LOOKING TOWARDS POOLES CORNER FROM 202 SOUTH

PHOTO 19



LOOKING NORTHBOUND ALONG 202 NORTH



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DOYLESTOWN TOWNSHIP

PREPARED BY:

Michael Baker

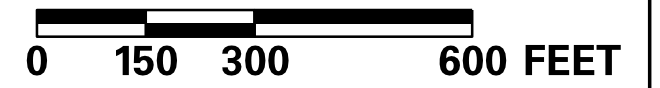
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SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
TRAIL FEASIBILITY STUDY

DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:

- PHOTO LOCATION
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 1A & 1B
- PROPOSED SHARED USE PATH/TRAIL ALTERNATIVE 2A & 2B



Public Survey Feedback and Results



Thank you! - Tell us what you think.

Thank you for viewing the 202 Trail Feasibility Study Virtual Public Meeting Room. Please rate your experience and provide additional comments below. Your feedback will help shape the future of this project.

...

1. Rate the Virtual Public Meeting



2. Are you supportive of this trail concept?

- Yes
- No
- Maybe (Explain in Question 4)

3. Which Trail alternative do you prefer? Refer to Alternative Alignments Map as a reference

- Red Route
- Yellow Route
- Blue Route
- Black Route

No Preference

Other

4. Voice any comments you may have regarding the future trail location

Enter your answer

5. Would you like us to explore an access point from your neighborhood to connect to this future trail?

Yes - I would love a new connection

No - I prefer my neighborhood to stay separated

6. Would you like to be kept up to date in the progression of this project? (If yes, please make sure to provide an email address in "other")

Yes

No

Other

Submit

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ID	Start time	Completion time	Name2	Address	Email Address	Organization (If Applicable)	Primary Interest in Trails
1	2/15/21 8:59:22	2/15/21 9:00:54	Geraldine Dougherty	114 Harvey Ave. Doylestown PA 18901	gdoc114@verizon.net		Walking;Biking;Getting to other Neighborhoods / Borough;The health rewards of just being in nature;
2	2/15/21 9:10:51	2/15/21 9:11:26 T		2157 Turk road	johnhogan82@gmail.com		Walking with Kids;Walking the Dog;Biking;Walking;Getting to other Neighborhoods / Borough;
3	2/15/21 9:12:28	2/15/21 9:13:10	Scott	191 East Rd	harriss27@yahoo.com		Walking the Dog;
4	2/15/21 9:14:21	2/15/21 9:15:23	Scott Harris	191 East Road, Doylestown, PA 18901	scott.harris3@sanofi.com		Walking;Biking;Walking the Dog;
5	2/15/21 9:15:58	2/15/21 9:16:52	MaryAnn Baenninger	17 Hickory Drive	Mabaenninger@gmail.com		Walking;Biking;Walking with Kids;Walking the Dog;Getting to other Neighborhoods / Borough;
6	2/15/21 9:21:38	2/15/21 9:22:21	Joe Shandlay	26 Creek drive	jshandlat@gmail.com		Walking the Dog;Biking;
7	2/15/21 9:25:23	2/15/21 9:26:03	Trish Maxson	218 Green St Doylestown	Trish.maxson@gmail.com		Walking;
8	2/15/21 9:50:18	2/15/21 9:52:29	D DAVISON	208 Lacey Ave	dbdvol@gmail.com		Biking;Walking;Walking with Kids;Getting to other Neighborhoods / Borough;getting to Delaware River;
9	2/15/21 9:58:05	2/15/21 10:01:06	Michael Dineen	338 East Ashland St., Doylestown, PA 18901	mdineen1210@gmail.com		Biking;Getting to other Neighborhoods / Borough;
10	2/15/21 10:10:50	2/15/21 10:11:56	Kathy and Kris Evans	175 Cherry Lane, Doylestown, PA 18901	kkglevans@gmail.com		Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
11	2/15/21 10:30:25	2/15/21 10:31:10	Curt Goldman	100 East State Street, Doylestown, PA 18901	curtgoldman@icloud.com		Biking;Electric Bikes;Getting to other Neighborhoods / Borough;Running;
12	2/15/21 10:44:21	2/15/21 10:44:50	Shannon Copenhaver	10 Charter Circle	shannon.e.copenhaver@gmail.com		Walking;Walking the Dog;
13	2/15/21 10:47:52	2/15/21 10:48:17	Tony Crenshaw	23 Walnut Lane	tcrenshaw@me.com		Biking;Walking the Dog;
14	2/15/21 10:51:05	2/15/21 10:52:03	Liz Wyckoff	7 Chestnut Dr	Elizabeth@quillandkey.net		Biking;Getting to other Neighborhoods / Borough;
15	2/15/21 10:55:41	2/15/21 10:56:10	Amy	Doylestown	Starlitac@aol.com		Biking;
16	2/15/21 11:00:05	2/15/21 11:01:03	Christine Johnson-Hall	89 S. Clinton Street Doylestown, PA 18901	cjhall89@verizon.net		Biking;Walking the Dog;
17	2/15/21 11:07:00	2/15/21 11:07:50	Pat Tagliolini	75 E Ashland St, Doylestown PA	pattrealtor@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;Walking with Kids;Walking the Dog;Electric Bikes;
18	2/15/21 11:09:08	2/15/21 11:09:13					
19	2/15/21 11:25:23	2/15/21 11:26:27	Karen Whitney	131 S. West St, Doylestown	kbwhitney59@gmail.com		Walking the Dog;Walking;Biking;Getting to other Neighborhoods / Borough;
20	2/15/21 11:34:22	2/15/21 11:35:24	Andrew Moyer	105 Greenbrook Court, New Hope, PA	andrew.f.moyer@gmail.com	I work at Delaware Valley University and have a strong interest in this effort.	Getting to other Neighborhoods / Borough;
21	2/15/21 11:42:42	2/15/21 11:44:12	Sharon R	305 Silver Linden Ln Chalfont	Msrigs@hotmail.com		Walking;Biking;

22	2/15/21 11:33:16	2/15/21 11:59:31	Rob and Claire Robinson	45 Radcliff Drive, Doylestown	newshoprobinson@gmail.com		Walking;Getting to other Neighborhoods / Borough;Walking the Dog;
23	2/15/21 12:00:36	2/15/21 12:01:37	Pam Byrne	3796 Swetland Dr	Pamm07@yahoo.com		Biking;
24	2/15/21 12:07:58	2/15/21 12:08:39	Eric Bader	101 N Clinton St	ewbader@verizon.net		Walking;Biking;
25	2/15/21 12:18:14	2/15/21 12:19:11	D Linde	426 Tally Ho Way	golfcoursetesting@gmail.com		Biking;
26	2/15/21 12:33:05	2/15/21 12:33:47	Jennifer Vicari	Iron Hill Road Doylestown	jennifervicari@hotmail.com		Walking;
27	2/15/21 12:33:47	2/15/21 12:34:36	Jack O'Brien	4 Hillcrest Circle	jjobrien48@verizon.net Forapa@centennialsd.org		Getting to other Neighborhoods / Borough;
28	2/15/21 12:52:51	2/15/21 12:54:41	Paula Foran	4522 Louise Saint Claire			Walking;
29	2/15/21 13:07:35	2/15/21 13:08:57	Steven Rock	306 W. Ashland St.	srock@att.net		Walking;Biking;
30	2/15/21 13:14:54	2/15/21 13:15:57	Jim Walter	1492 Sugar Bottom Road Furlong PA	jwalter881@verizon.net		Walking;
31	2/15/21 13:27:24	2/15/21 13:28:11	Susan Schwartz	204 Woodspring Circle, Doylestown, PA 18901	suzycat1@comcast.net		Walking;Biking;Walking the Dog;
32	2/15/21 13:30:50	2/15/21 13:31:58	Bill Wert	17 Blythewood Road, Doylestown, 18901	wvu95@yahoo.com	Doylestown Bike & Hike Committee	Walking;Biking;
33	2/15/21 13:33:58	2/15/21 13:35:13	Jean A Rollo	165 Decatur Street, Doylestown PA 18901	rollje@comcast.net		Walking;Biking;Walking with Kids;Walking the Dog;Electric Bikes;
34	2/15/21 14:17:35	2/15/21 14:18:31	Laura Morocco	65 N Hamilton st doylestown	Morocco.laura@gmail.com		Walking;Getting to other Neighborhoods / Borough;
35	2/15/21 14:19:27	2/15/21 14:20:19	Paul Hale	109 Steeplechase Drive Doylestown PA 18901	phale24@comcast.net		Walking;Biking;
36	2/15/21 14:35:33	2/15/21 14:37:38	Ann Toole	6081 Honey Hollow Rd Doylestown, PA	anntoole@icloud.com		Doylestown to New Hope;
37	2/15/21 14:39:31	2/15/21 14:40:17	Lisa Farina	46 Bridge St., Doylestown, PA 18901	lisafarina1@gmail.com		Biking;
38	2/15/21 14:49:00	2/15/21 14:52:08	Gary Harris	34 Crestland Terrace, Doylestown, PA 18901	gharris@doylestownborough.net	Doylestown Borough Recreation Director	Walking;Getting to other Neighborhoods / Borough;
39	2/15/21 14:58:22	2/15/21 14:59:36	Matt Wufsus	629 old elm street, conshohocken, pa 19428	mattwufu@gmail.com		Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
40	2/15/21 15:08:03	2/15/21 15:08:32					
41	2/15/21 15:35:40	2/15/21 15:36:56	Stephanie Ferraro	11 Chestnut Dr, Doylestown, PA	Stephanieferraro@me.com		Walking;Walking with Kids;Walking the Dog;Getting to other Neighborhoods / Borough;
42	2/15/21 15:44:53	2/15/21 15:45:56	Stephanie Parady	279 West court St, Doylestown	theparadys@verizon.net		Walking;
43	2/15/21 15:47:06	2/15/21 15:47:16					
44	2/15/21 15:46:16	2/15/21 15:47:36	Trish Isaacson	91 Williams Drive Fountainville Pa 18923	trish57@comcast.net		Walking;Biking;Walking the Dog;
45	2/15/21 15:47:37	2/15/21 15:47:42					
46	2/15/21 16:13:24	2/15/21 16:14:21	Maggi boyer	305 Dorset Ct	Maggiboyer46@gmail.com		Biking;Walking;
47	2/15/21 16:54:58	2/15/21 16:56:08	Linda Miller	219 Hillcrest Drive, Doylestown PA 18901	for4millers@gmail.com		Biking;Getting to other Neighborhoods / Borough;
48	2/15/21 17:04:12	2/15/21 17:00:55	Jane Neal	2088 Turk Road, Doylestown PA 18901	janeneal09@gmail.com		Walking;Biking;
49	2/15/21 18:12:13	2/15/21 18:13:07	Marie Carota	1255 Pebble Hill Rd	madpie@verizon.net		Walking;Biking;
50	2/15/21 18:56:42	2/15/21 18:58:05	Debra Campbell	168 Lafayette St Doylestown PA 18901	debracampbell168@comcast.net		Walking;Biking;

51	2/15/21 19:18:55	2/15/21 19:19:48 Lynn Goldman	860 pebble hill road	lynn@goldmanlawoffices.com	Bike/Hike comm	Walking;Biking;Electric Bikes;Getting to other Neighborhoods / Borough;
52	2/15/21 20:10:58	2/15/21 20:11:31 Laurie	439 pine run rd	lbwilson439@gmail.com		Walking;Biking;Walking with Kids;Walking the Dog;
53	2/15/21 20:13:01	2/15/21 20:13:58 Margaret MacDonald	219 East Rd	Npjj@verizin.net		Walking;Biking;Walking the Dog;
54	2/15/21 23:20:15	2/15/21 23:21:20 Joe Poretta	543 Maple Ave. Doylestown Pa 18901	Joe.Poretta@gmail.com		Biking;Walking with Kids;
55	2/16/21 6:53:52	2/16/21 6:56:54 David Dowdy	40 Stringer Dr	ddinc1979@yahoo.com		Walking;
56	2/16/21 8:33:31	2/16/21 8:34:19 Linda Salkovitz	174 Lafayette St., Doylestown, PA 18901	ljsalk@comcast.net	Central Bucks Bicycle Club	Biking;
57	2/16/21 8:42:26	2/16/21 8:43:50 Jim Houghton	991 New Galena Rd, Doylestown, PA 18901	Jeh0708@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
58	2/16/21 9:08:03	2/16/21 9:08:42 chris stanford	909 harcourt ln, harleysville pa	cstanford@mbakerintl.com		Biking;
59	2/16/21 9:15:14	2/16/21 9:15:59 Jesse Roy	2 Orchard Lane	jumpingjesse1@gmail.com		Walking;Biking;Walking with Kids;
60	2/16/21 9:17:04	2/16/21 9:18:11 Dan Clark	28 Juniper Drive	dgordonclark@gmail.com		Walking with Kids;Biking;
61	2/16/21 9:23:11	2/16/21 9:23:55 Jessica Pollner	1 Chestnut Drive Doylestown PA18901	jspollner@yahoo.com		Walking;Biking;Walking with Kids;Walking the Dog;
62	2/16/21 10:58:32	2/16/21 10:59:17 Sharon Yerkes	26 Morgan Hill Drive. Doylestown, PA 18901	siyerkes@comcast.net		Walking;Walking the Dog;Getting to other Neighborhoods / Borough;
63	2/16/21 11:05:38	2/16/21 11:06:48 dana barnett	466 maple avenue	jmail4danabarnett@gmail.com		Biking;
64	2/16/21 13:34:06	2/16/21 13:35:04 Amanda	419 S main st sellersville PA	krameramanda10@gmail.com		Biking;
65	2/16/21 15:06:07	2/16/21 15:07:34 Barbara Goetz	303 Jasper Ct. Warrington, PA 18976	Bmonty1011@yahoo.com		Walking;Biking;Walking the Dog;Walking with Kids;
66	2/16/21 15:06:46	2/16/21 15:07:47 Lou Ferere		louipo2000@aol.com		Biking;Walking;
67	2/16/21 15:30:23	2/16/21 15:33:41 Egil Nilsson	139 south main steet	egil_n@hotmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
68	2/16/21 15:54:54	2/16/21 15:55:50 Brian Boger	35 N. Franklin St., Doylestown, PA 18901	brian@bikeworks.shop	Bike Works, LLC	Biking;Walking;Electric Bikes;Walking the Dog;
69	2/16/21 16:30:14	2/16/21 16:30:56 Kim Casale	709 West Sandy Ridge Rd	Kcasale@qcsd.org		Biking;Getting to other Neighborhoods / Borough;
70	2/16/21 17:10:17	2/16/21 17:11:25 Larry Oxman	Warrington, PA	larry@team-ox.org	Team Ox Cycling	Biking;
71	2/16/21 19:49:12	2/16/21 19:52:18 Mark Allen	325 Horseshoe Way, Doylestown, PA	Amarmel1@gmail.com		Biking;Walking the Dog;
72	2/16/21 20:10:40	2/16/21 20:11:36 Brent Zellner	544 New Britain Rd	brentzellner@icloud.com		Walking;Biking;Walking with Kids;Walking the Dog;Electric Bikes;Getting to other Neighborhoods / Borough;
73	2/16/21 20:19:35	2/16/21 20:21:00 David Inscho	145 Homestead Drive Doylestown PA 18901	davidinscho@gmail.com		Walking with Kids;Biking;Walking the Dog;
74	2/16/21 20:31:10	2/16/21 20:32:10 Amy Mazzanti	142 Hart Ave, Doylestown, PA 18901	amazanti1@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
75	2/16/21 21:09:25	2/16/21 21:10:33 Vicki Fenstermaker	14 Walnut Ln, Doylestown PA	vfmaker@gmail.com		Biking;
76	2/16/21 21:36:27	2/16/21 21:37:02 Jan Cope	196 Green Street, Doylestown 18901	jancope@verizon.net		Walking;Walking with Kids;
77	2/17/21 1:52:17	2/17/21 1:54:08 Chip Krauss	141 Hart Avenue Doylestown PA 18901	Cakraussiii@comcast.net		Walking;
78	2/17/21 10:23:31	2/17/21 10:25:21 M K Bingle	349 Linden Ave, Doylestown 18901	mkbingle@msn.com		Walking;Getting to other Neighborhoods / Borough;Biking;

79	2/17/21 10:38:19	2/17/21 10:39:29 C R Moore	3511 Windridge Dr, Doylestown, PA 18902	cnrlme@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
80	2/17/21 11:12:14	2/17/21 11:12:55 Ken Snyder	50 Foxcroft Drive Doylestown	kensnyder3@me.com		Walking the Dog;
81	2/17/21 12:22:07	2/17/21 12:23:02 Michael Hermab	3933 Captain Molly Circle Doylestown Pa 18902	Michaelfherman@gmail.com		Biking;
82	2/17/21 12:34:52	2/17/21 12:36:29 Kathy Brown	193 Sunset View Dr Doylestown PA 18901	fkrpbrown@comcast.net	Dtown P&R Board, Dtown resident	Walking;Walking the Dog;Walking with Kids;Getting to other Neighborhoods / Borough;
83	2/17/21 13:52:32	2/17/21 14:01:42 Bruce Salkovitz	174 Lafayette St	bsalkovitz@gmail.com		Biking;
84	2/18/21 3:04:15	2/18/21 3:06:00 Lisa				Walking;
85	2/18/21 8:45:22	2/18/21 8:46:11 john skorupa	202 n. clinton street	johskor@comcast.net		Walking;Biking;
86	2/18/21 8:45:44	2/18/21 8:47:04 Jennifer Herring	19 Foxcroft Drive Doylestown Pa 18901	Jennifervherring@gmail.com	Doylestown Township	Walking;Biking;Walking with Kids;Walking the Dog;Electric Bikes;Getting to other Neighborhoods / Borough;
87	2/18/21 10:35:43	2/18/21 10:36:50 Jason Forsell	64 Steeplechase Dr Doylestown	jasonforsell@gmail.com		Biking;Getting to other Neighborhoods / Borough;
88	2/18/21 13:59:40	2/18/21 14:00:28 John Hall	89 S. Clinton St., Doylestown, PA 18901	jphall91@gmail.com		Biking;
89	2/18/21 15:32:36	2/18/21 15:33:08 Rebecca Custer	6 Bridgepoint Court	rebecca.custer77@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
90	2/18/21 15:52:46	2/18/21 15:53:12 Brennan Mault	6 Bridgepoint Ct	bmault1775@gmail.com		Walking;Getting to other Neighborhoods / Borough;
91	2/18/21 19:04:10	2/18/21 19:05:01 Jan Solkov	206 Eagle Lane, Doylestown, PA 18901	drjanice1@yahoo.com		Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
92	2/18/21 21:55:46	2/18/21 21:56:45 Terri	6 Easthill Drive	durkinterri@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
93	2/21/21 11:01:28	2/21/21 11:05:51 Ellen Phillips	61 Green Tree Dr. Doylestown	Phillipsem61@gmail.com		Walking;Getting to other Neighborhoods / Borough;
94	2/21/21 11:08:07	2/21/21 11:08:48 Walter Mlodzinski	469 Linden Ave Doylestown PA 18901	walter.mlodzinski@comcast.net		Walking;Biking;Getting to other Neighborhoods / Borough;
95	2/21/21 11:08:33	2/21/21 11:09:10 Ellen Phillips	61 Green Tree Dr.	Phillipsem61@gmail.com		Walking;Getting to other Neighborhoods / Borough;
96	2/21/21 15:03:45	2/21/21 15:05:26 Mary Lou Parry	361 Birdsong Way, Doylestown PA 18901	marylouparry@comcast.net		Walking;Getting to other Neighborhoods / Borough;
97	2/22/21 12:22:27	2/22/21 12:23:29 Nancy Santacecilia	24 Mystic View lane Doylestown Pa 18901	nmsantacecilia@msn.com		Walking the Dog;Walking;
98	2/22/21 15:19:02	2/22/21 15:20:03 doug pedersen	5234 bailey court n. doylestown, pa 18902	thepedersens01@comcast.net		Walking;Biking;Walking with Kids;
99	2/23/21 8:54:04	2/23/21 8:54:27 Kris	90 Mercer Ave	kris@mindyour.design	Resident	Walking the Dog;Walking;Biking;
100	2/23/21 8:57:56	2/23/21 8:58:35 Paula Campbell	28 Morgan Hill Drive	camppr@gmail.com		Walking;Biking;Walking the Dog;Electric Bikes;Getting to other Neighborhoods / Borough;

101	2/23/21 9:26:03	2/23/21 9:26:44	Christine Sheaffer	60 quarry road, doylestown, pa 18901	cgrandzol@yahoo.com	N/A	Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
102	2/23/21 9:31:20	2/23/21 9:32:00	Robert O'Reilly	14 Hickory Drive	nineoften@gmail.com		Walking;Biking;Walking the Dog;Walking with Kids;Electric Bikes;Getting to other Neighborhoods / Borough;
103	2/23/21 9:32:07	2/23/21 9:33:06	Michelle Crunkleton	219 W Court St., Doylestown, PA 18901	mcrunkle@yahoo.com		Walking;Biking;Getting to other Neighborhoods / Borough;
104	2/23/21 9:46:49	2/23/21 9:47:36	Robert Hutchison	334 Maple Ave	maplehutch@me.com		Walking;Biking;Walking the Dog;
105	2/23/21 10:42:54	2/23/21 10:44:05	Kim O'Byrne	1803 Ticonderoga Dr. Jamison	nibhroin12@gmail.com		Biking;Walking;
106	2/23/21 11:04:03	2/23/21 11:05:47	Robert Purdy	3372 Byron Drive, Doylestown, PA 18902	rgpurdy19@gmail.com	Purdy Consulting	Walking;Biking;Getting to other Neighborhoods / Borough;Being outside.;
107	2/23/21 11:21:42	2/23/21 11:23:18	Lisa Moscherosch	106 East Oakland Avenue	lmoscherosch1@gmail.com		Biking;
108	2/23/21 11:23:27	2/23/21 11:24:18	Michael Moscherosch	106 East Oakland Ave	lmoscherosch1@gmail.com		Biking;
109	2/23/21 11:45:12	2/23/21 11:48:47	Brian Emig	203 W Sandy Ridge Road	Bcemig@gmail.com		Walking;Walking the Dog;Getting to other Neighborhoods / Borough;Biking;Nature exploration ;
110	2/23/21 11:56:13	2/23/21 11:57:42	William Burdett	44 Steeplechase Dr	William.j.burdett@gmail.com		Walking;Biking;Walking with Kids;
111	2/23/21 11:59:59	2/23/21 12:00:34	Rebecca Lea Barlow	4057 Landisville rd	rbarlow@verizon.net		Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
112	2/23/21 12:02:28	2/23/21 12:04:16	Denise Blasdale	34 N. Church Street	Denise.blasdale@verizon.net		Walking;Biking;
113	2/23/21 12:53:37	2/23/21 12:54:07	John Case	4106 Meridian Boulevard	jcase1984@icloud.com	none	Biking;
114	2/23/21 12:57:47	2/23/21 12:59:29	BRUCE HOCHMAN	19 CHESTNUT DRIVE, DOYLESTOWN PA 18901	BRUCEHOCHMAN@GM AIL.COM	DOYLESTOWN BIKE AND HIKE	Biking;
115	2/23/21 13:00:53	2/23/21 13:01:42	Louis Ricciardi	2206 Pleasant Avenue, Glenside, PA 19038	lcr1965@gmail.com		Walking;
116	2/23/21 13:30:10	2/23/21 13:30:58	Steve Nelson	103 East St, Doylestown	stevenelson@comcast.net		Biking;
117	2/23/21 13:31:13	2/23/21 13:32:57	John ripley	64 hillside ave 18901	Chef353@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
118	2/23/21 13:31:59	2/23/21 13:33:20	Julia Ripley	64 Hillside Ave Doylestown PA 18901	juliamripley@gmail.com		Walking;
119	2/23/21 14:23:49	2/23/21 14:25:25	Michael kracht	1051 ferry road, doylestown	Mkracht@wkclaw. Bet		Walking;
120	2/23/21 14:42:36	2/23/21 14:43:22	Matt Senior	114 Cottonwood Court	mattsenior123@gmail.com		Biking;Getting to other Neighborhoods / Borough;
121	2/23/21 14:57:53	2/23/21 14:59:58	Charles Stahl	3640 Clay Road Doylestown PA 18902	Cjstahl@gmail.com		Biking;Walking;
122	2/23/21 17:23:49	2/23/21 17:25:14	Wendy Kennedy	2720 Kent Dr Doylestown, PA 18902	Kennedy4170@comcast.net		Getting to other Neighborhoods / Borough;Walking;Biking;
123	2/23/21 18:57:51	2/23/21 18:58:04					
124	2/23/21 19:32:55	2/23/21 19:34:20	William Burdett	44 Steeplechase Dr	William.j.burdett@gmail.com		Walking;Biking;Walking with Kids;
125	2/23/21 19:37:41	2/23/21 19:38:50	Cynthia	Hollis	like2knitnow@yahoo.com		Walking;Biking;Walking with Kids;Walking the Dog;

126	2/23/21 20:17:35	2/23/21 20:19:26 Ali Shalchi	4288 Milords Lane Doylestown 18902 and 18 East Oakland avenue Doylestown 18901	Tuc41509@temple.edu Jeannekeagy@gmail.com	DreamLine	Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
127	2/23/21 20:46:24	2/23/21 20:47:26 Jeanne Keagy	29 Endslo Lane	m		Biking;
128	2/23/21 22:14:01	2/23/21 22:15:11 Paul Hernandez	4451june meadow dr	ph5466c@gmail.com		Biking;
129	2/23/21 22:16:13	2/23/21 22:16:27				
130	2/24/21 6:17:17	2/24/21 6:18:24 Krista Harper	90 W Sandy Ridge Rd, Doylestown	Krista@harperbusinessl aw.com		Walking;Biking;Getting to other Neighborhoods / Borough;
131	2/24/21 7:11:18	2/24/21 7:12:59 Joan Merkel	166 Wood Street Doyestown PA 18901	joanmerkekl166@gmail .com		Walking;Biking;
132	2/24/21 7:30:32	2/24/21 7:32:43 lisa s	4976 grundy way doylestown	lcsathome@gmail.com		Walking;Biking;Walking with Kids;Getting to other Neighborhoods / Borough;
133	2/24/21 10:32:37	2/24/21 10:33:51 Andrea	2898 Mill Road	alstrout_2000@yahoo. com		Walking;
134	2/24/21 10:45:06	2/24/21 10:46:45 Diane L. Smith	2076 Holicon Rd. New Hope, PA 18938	dianesmith@bcas.org jshuportyaka@gmail.co m	Buckingham Township EAC and Bucks County Audubon Society	Walking;Walking the Dog;Getting to other Neighborhoods / Borough;
135	2/24/21 10:46:17	2/24/21 10:47:00 Jenya Shuportyaka	23 Edison Rd. - C			Walking;Walking the Dog;roller skating;
136	2/24/21 11:29:26	2/24/21 11:32:12 Victoria kinnaird	87 Elfman drive	Kinnaird.victoria@gmai l.com		Walking the Dog;Getting to other Neighborhoods / Borough;Walking with Kids;Walking;
137	2/24/21 12:43:23	2/24/21 12:44:05 Dan Wood	71 Duane Rd Doylestown PA 18901	dwood@doylestownpa. org	Doylestown Township	Walking;Biking;Walking with Kids;
138	2/24/21 13:57:41	2/24/21 13:58:49 Bill Hogenauer	203 Windsor Way Doylestown, PA 18901	bill@hogenauer.com Judy@HendrixsonArchi tects.com		Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;Enjoying the outdoors safely without auto traffic;
139	2/24/21 14:07:39	2/24/21 14:08:30 Judy Hendrixson	243 Almshouse Road Burnt House Hill Rd.		Doylestown Bike & Hike	Walking;Walking the Dog;Biking;
140	2/24/21 15:27:26	2/24/21 15:28:37 Andy	Doylestown 18902	asjeam@gmail.com	None	Walking with Kids;
141	2/24/21 16:21:25	2/24/21 16:24:08 Art Wawiernia	213 Creek Road, Doylestown, PA 18901 (New Britain Township)	art.wawiernia@gmail.c om	Not only am I a Doylestown community resident, I am also a Water Resource Engineer with Michael Baker International	Biking;Getting to other Neighborhoods / Borough;
142	2/24/21 17:30:26	2/24/21 17:31:53 Phil Hoke	27 Meadow Lane, Doylestown, PA 18901	phil.hoke@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
143	2/25/21 9:00:27	2/25/21 9:01:20 Ed	2931 snake hill rd	ed3070@gmail.com		Biking;
144	2/25/21 9:53:21	2/25/21 9:54:16 Robert McEwan	3363 Windy Bush Rd.	robertandmariamcewa n@gmail.com		Walking;Biking;
145	2/25/21 10:42:41	2/25/21 10:43:40 Nancy	PO box 51 Carversville PA	nstockallen@comcast.n et	none	Walking;Biking;Getting to other Neighborhoods / Borough;Connecting to the Delaware River;
146	2/25/21 13:57:20	2/25/21 13:58:08 Megan Fraser	58 Taylor Avenue, Doylestown	megan@frasers.org		Walking;Biking;Walking the Dog;Getting to other Neighborhoods / Borough;
147	2/25/21 15:27:47	2/25/21 15:28:47 David Zipf	Crocker lane, Jamison, Pa	dacarzi@aol.com		Biking;Walking;Getting to other Neighborhoods / Borough;
148	2/25/21 17:11:53	2/25/21 17:12:46 Evan				Biking;Walking;
149	2/25/21 18:08:44	2/25/21 18:08:58				Biking;Walking;Getting to other Neighborhoods / Borough;

150	2/25/21 18:37:41	2/25/21 18:38:16 Len Mazzanti	142 Hart Ave	lenmazz@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
151	2/25/21 22:01:37	2/25/21 22:02:20 Cassie Galster	545 E. State Street	cassielynn.galster@gmail.com		Biking;Getting to other Neighborhoods / Borough;
152	2/26/21 5:34:19	2/26/21 5:35:14 Cindy Wallace	95 Springs Drive	cwallace55@comcast.net		Walking;Biking;
153	2/26/21 6:55:57	2/26/21 6:58:29 Jim Resek	286 W. Oakland Avenue, Doylestown, PA 18901	jresek@aol.com		Biking;Walking;Getting to other Neighborhoods / Borough;
154	2/26/21 8:22:28	2/26/21 8:22:51 Jill Schweizer	222 Doyle Street	jillschweizer@verizon.net		Walking;Walking the Dog;
155	2/26/21 8:28:56	2/26/21 8:29:29 Sean Maloney	526 Cornell Drive, Warrington, PA 18976	sean.f.maloney@gmail.com	n/a	Biking;Walking with Kids;
156	2/26/21 9:13:40	2/26/21 9:14:51 Christine Weiss	26 Pebble Valley Drive, Doylestown, PA 18901	chrisbrenweiss@gmail.com		Getting to other Neighborhoods / Borough;Walking;Biking;
157	2/26/21 9:33:26	2/26/21 9:34:52 Paul Bizon	210 West Fairwood Drive, Chalfont, PA 18914	pbizon@riggsdistler.com		Walking;Biking;
158	2/26/21 10:08:30	2/26/21 10:10:01 Diane Hendler	6 Sunrise Drive, Doylestown, PA 18901	dianeshendler@gmail.com	(Belong to the Women Bike Dtown FB group)	Walking;Biking;Getting to other Neighborhoods / Borough;
159	2/26/21 10:15:37	2/26/21 10:16:41 John Foss	2-31 Aspen Way, Doylestown, PA 18901-2741	jfoss9@msn.com		Walking;Biking;
160	2/25/21 17:16:53	2/26/21 10:41:08 Louise Silberg	3126 Cloverly Drive Furlong Pa. 18925	gardenlass50@gmail.com	Buckingham Township	Biking;Getting to other Neighborhoods / Borough;Walking;
161	2/26/21 11:03:53	2/26/21 11:04:22 Tim Sheroff	4095 Gregory Drive, Doylestown PA 18902	tsheroff@gmail.com		Biking;Walking with Kids;
162	2/26/21 11:13:29	2/26/21 11:20:57 Joe Keffer	81 Johanna Drive Holland, PA 18966	joe@bikeworks.shop	Bike Works	Biking;
163	2/26/21 12:01:56	2/26/21 12:03:33 Scott Loder	223 Chatham Place Lansdale, PA	sloder@comcast.net		Biking;Walking;
164	2/26/21 12:37:23	2/26/21 12:47:12 andy everett	1157 sandy ridge rd	andyeverett57@gmail.com		Biking;
165	2/26/21 14:54:22	2/26/21 14:55:41 Stanley Dunn	62 Townview Dr	dunnsed@gmail.com		Walking;
166	2/26/21 18:26:40	2/26/21 18:27:27 Alaina Tweddale	49 Steeplechase Drive, Doylestown, PA 18901	alaina9927@gmail.com		Walking;Biking;Walking with Kids;Walking the Dog;Getting to other Neighborhoods / Borough;jogging;
167	2/26/21 19:07:35	2/26/21 19:08:47 David Rohlfing	4516 Louise Saint Claire Dr. Doylestown, PA 18902	drohlfm@hotmail.com		Biking;Getting to other Neighborhoods / Borough;
168	2/26/21 20:05:13	2/26/21 20:07:37 Rick Gaver	201 Roxbury Road, Shippensburg, Pa. 17257	rickgaver42@yahoo.com		Electric Bikes;
169	2/26/21 20:08:26	2/26/21 20:08:34				
170	2/26/21 20:35:24	2/26/21 20:36:46 Phil Roth	Blooming Glen PA	roth822@verizon.net	none	Biking;
171	2/26/21 20:37:03	2/26/21 20:37:18				
172	2/26/21 21:09:24	2/26/21 21:11:03 Kimberly Nelson	103 East Street, Doylestown, PA 18901	kdsn103@gmail.com		Biking;
173	2/27/21 7:27:49	2/27/21 7:29:02 Larry Gage	7 Oak Drive, Doylestown, PA 18901	larrygage@verizon.net		Walking;Biking;Getting to other Neighborhoods / Borough;
174	2/27/21 7:52:48	2/27/21 7:53:36 Mike McCormack	107 Warden Rd., Doylestown PA 18901	mikemccormack18901@gmail.com		Walking;Biking;Getting to other Neighborhoods / Borough;
175	2/27/21 8:45:54	2/27/21 8:47:13 James McDonald	475 Ferry Rd	jmcdonald507@gmail.com		Walking;Biking;
176	2/27/21 10:07:37	2/27/21 10:08:29 Paul Rehmet	71 Brinker Drive, Doylestown	paul.rehmet@gmail.com		Walking;
177	2/27/21 10:28:10	2/27/21 10:29:11 Raymond Cartee	11 Scott Road, Doylestown, Pa. 18901	raycartee@msn.com		Walking;Biking;Walking the Dog;

178	2/27/21 10:58:27	2/27/21 10:59:17 Pitts family	53 Steeplechase Drive Doylestown PA 18901	BLBULK@verizon.net	Biking;Walking the Dog;Walking;
179	2/27/21 11:38:19	2/27/21 11:47:17 Steve Fleischut	63 Woodcrest Lane, Doylestown PA 18901	stevefly@comcast.net	Biking;Walking;Walking with Kids;Walking the Dog;Getting to other Neighborhoods / Borough;site seeing / travel / nature;
180	2/27/21 19:40:28	2/27/21 19:41:48 Linda Luebke	133 Tower Hill Road 3941 Fountain Circle, Fountainville	laluebke@msn.com Janicesthomas@verizon.net	concerned about wetlands;
181	2/28/21 6:19:52	2/28/21 6:20:38 Janice Thomas	2250 S Easton Rd Doylestown	eshapiro.pa@gmail.com	Walking the Dog;Walking;
182	2/28/21 9:33:55	2/28/21 9:34:40 Eileen Shapiro	2990 Yorkshire Road Doylestown 18902	jockesh@verizon.net	Walking;Biking;
183	2/28/21 11:01:11	2/28/21 11:02:11 Curt Eshleman	Doylestown Boro	rpatte@comcast.net	Biking;
184	2/28/21 11:12:22	2/28/21 11:13:45 dick patterson	Doylestown		Walking;Biking;Running;
185	2/28/21 11:28:02	2/28/21 11:28:54 Hal			Walking;Biking;
186	2/28/21 14:09:27	2/28/21 14:10:03 Stephen Ayres	25 Woodbridge Dr	uphigh@gmail.com	Biking;Getting to other Neighborhoods / Borough;Walking;
187	2/28/21 14:43:32	2/28/21 14:45:03 Paula Lichtenstein	3725 Christopher Day Road, Doylestown PA 18902	paulal321@gmail.com	Walking;Getting to other Neighborhoods / Borough;
188	2/28/21 15:31:56	2/28/21 15:32:34 Adam Luebke	35 Elm street Fredonia NY	aluebke@gmail.com	Biking;
189	2/28/21 15:46:24	2/28/21 15:47:10 Edward Denton	24 Foxcroft Drive, Doylestown Township	eddenton3@verizon.net self	Walking;Biking;
190	2/28/21 15:49:55	2/28/21 15:52:14 Susan Romanus	485 Pebble Hill Road 435 Maple Avenue, Doylestown	sromanus@verizon.net jtlasala@gmail.com	Impact on my property;
191	2/28/21 18:35:52	2/28/21 18:36:32 John LaSala	6081 Honey Hollow Rd Doylestown, PA 18902	anntoole@comcast.net	Biking;
192	2/28/21 19:16:30	2/28/21 19:18:03 Ann Toole	483 Pebble Hill Road, Doylestown, PA 18901	rick@rickskidmore.com	Biking;
193	3/1/21 7:19:16	3/1/21 7:19:55 Rick Skidmore			Walking;Biking;
194	3/1/21 9:30:26	3/1/21 9:31:36 Ken Fuller	2219 Blue Stem Drive	fullerken8@gmail.com	Walking;Biking;Getting to other Neighborhoods / Borough;
195	3/1/21 14:35:00	3/1/21 14:36:00 Paul Hrrnandez-Cuebas	4451 June Meadow dr	ph5466c@gmail.com	Biking;Walking the Dog;
196	3/2/21 10:20:22	3/2/21 10:20:57 Bobbi McGlynn	246 Avenue A	hipthelma@gmail.com	Biking;Walking;Getting to other Neighborhoods / Borough;
197	3/2/21 19:34:02	3/2/21 19:35:35 Richard Olsen	131 Tower Hill Rd	richard.olsen@verizon.net	Walking;Biking;
198	3/5/21 8:40:15	3/5/21 8:43:39 Maureen Brill	3986 Spring Valley Rd Foylestown, PA 18902	maureenfloodbrill@gmail.com	Walking;Walking with Kids;Getting to other Neighborhoods / Borough;
199	3/5/21 10:10:23	3/5/21 10:11:04 Tamara	270 Paine St	Tamara.k.nicholson@gmail.com	Walking;Biking;
200	3/5/21 13:31:09	3/5/21 13:32:58 P	W	pwoodward@weidel.com	Walking;Biking;Walking the Dog;Walking with Kids;Electric Bikes;Getting to other Neighborhoods / Borough;
201	3/7/21 10:27:59	3/7/21 10:29:02 victoria myers	686 stryker avenue	vickymyers07@gmail.com	Biking;Walking;
202	3/7/21 10:29:25	3/7/21 10:31:23 victoria myers	686 stryker avenue	vickymyers07@gmail.com	Biking;Walking;Electric Bikes;We are Senior citizens who have biked during the pandemic and would welcome this extension;
203	3/15/21 23:30:42	3/15/21 23:31:49 Leslie MorrisSmith	8818 Duveen Dr	leslm5791@gmail.com	Biking;

204	4/5/21 20:53:13	4/5/21 20:54:20 Greg Hoffman	451 Wells Rd Doylestown, PA 18901	ghoff1966@gmail.com	Biking;
205	4/10/21 14:01:46	4/10/21 14:02:26 judith fraivillig	387 Linden Ave, Doylestown	judith.fraivillig@gmail.com	Walking;Getting to other Neighborhoods / Borough;
206	6/3/21 15:30:36	6/3/21 15:30:59 Tiffany Schmidt	446 Wells Road	TiffanyASchmidt@yahoo.com	Walking with Kids;Walking the Dog;Walking;



Welcome! Tell us about yourself!

206
Responses

07:55
Average time to complete

Active
Status

1. Name

197
Responses

Latest Responses

"Tiffany Schmidt"

"judith fraivillig"

"Greg Hoffman"

2. Address

194
Responses

Latest Responses

"446 Wells Road"

"387 Linden Ave, Doylestown"

"451 Wells Rd Doylestown, PA 18901"

3. Email Address

194
Responses

Latest Responses

"TiffanyASchmidt@yahoo.com"

"judith.fraivillig@gmail.com"

"ghoff1966@gmail.com"

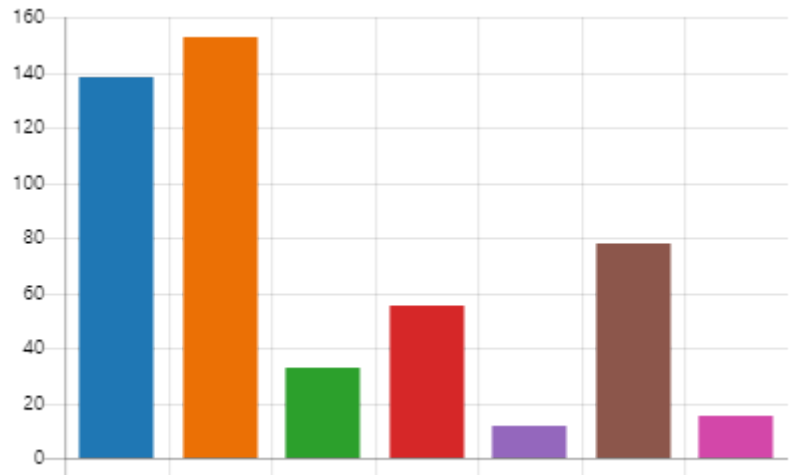
4. Organization (If Applicable)

27
Responses

Latest Responses

5. Primary Interest in Trails

Walking	138
Biking	153
Walking with Kids	33
Walking the Dog	55
Electric Bikes	12
Getting to other Neighborhoo...	78
Other	15



ID	Start time	Completion time	Rate the Virtual Public Meeting	Are you supportive of this trail concept?	Which Trail alternative do you prefer? Refer to Alternative Alignments Map as a reference	Voice any comments you may have regarding the future trail location	Would you like us to explore an access point from your neighborhood to connect to this future trail?	Would you like to be kept up to date in the progression of this project? (If yes, please make sure to provide an email address in "other")
4	2/13/21 0:23:47	2/13/21 0:43:40	5	Yes	Yellow Route;	I live at 544 NBR Rd. Just an observation, currently trail users must use my neighbors (542) driveway easement and occasionally mine to cut from the path to access the NBR overpass bridge towards central park. Safety mods to cross the bridge would be greatly appreciated for taking my kids to the castle. I also think linking this path down and around (Warden) over to the new wawa is great too. Also, with the new senior living building being built in this area, being able to create a "circle" (easton rd, NBR, Warden) would benefit those new residents as well. My only concern would be any removal of trees along the 611 bypass for sound pollution reasons. Particularly in that skinny section of tree buffer (left side of photo #9). My family would greatly enjoy these improvements!	Yes - I would love a new connection	Yes;
5	2/15/21 9:39:53	2/15/21 9:43:35	5	Yes	Black Route;	The black route is straight forward with the least issues. Views of golf course vs highway are preferred. Don't let golf balls be a determining driver, the golf course is responsible for keeping their balls on their property, fencing should be their responsibility.	Yes - I would love a new connection	No;
6	2/15/21 9:45:59	2/15/21 9:49:41	5	Yes	Red Route;	This is fabulous. My partner and I are avid walkers in the borough and have dodged more cars on narrow roads than we care to contemplate. We deeply appreciate the current trail system, and have been wondering when and if we might expect expansions. Personally I would like to see a trail along Pebble Hill to Edison-Furlong (and continuing on Pebble Hill after Sugarbottom), but if this is next on the priority list, so be it!	Yes - I would love a new connection	Yes;Trish.maxson@gmail.com;
7	2/15/21 10:01:40	2/15/21 10:04:13	5	Yes	Black Route;	I like the potential for the trail to be along the golf course, but does come with some concerns for safety and may include a lot of coordination with the club.	Yes - I would love a new connection	Yes;
8	2/15/21 10:01:01	2/15/21 10:04:55	3	No	Red Route;	No matter which trail you pick, the light at State St and Bypass needs to be addressed. In my opinion it will need a left turn arrow to turn from bypass on to State St in order to make it safe for pedestrians and bicyclists.	No - I prefer my neighborhood to stay separated	Yes;grady5@verizon.net;
9	2/15/21 10:06:04	2/15/21 10:07:27	3	Yes	All of them;		Yes - I would love a	Yes;
10	2/15/21 10:22:39	2/15/21 10:28:50	5	Yes	Red Route;Blue Route;	Please encourage Buckingham and Plumstead to add to the connectivity, would like to be able to connect to walking path on Swamp road to 413, also eventually to the Canal Path.	Yes - I would love a new connection	Yes;tonyd1411@yahoo.com;
11	2/15/21 10:28:33	2/15/21 10:29:26	5	Yes	No Preference;		Yes - I would love a	Yes;Annemonoky@gmail.com;
12	2/15/21 10:58:52	2/15/21 11:06:02	5	Yes	Blue Route;	Thank you for planning this and seeking our input. We use the existing trail a great deal for walking the dog, walking with friends, and especially biking. We deeply appreciate this project and the access, esp. during the pandemic. As to your question 5 below, we can easily access the trail right now by riding a few blocks through the borough.	No - I prefer my neighborhood to stay separated	Yes;
13	2/15/21 11:39:35	2/15/21 11:40:42	4	Yes	Yellow Route;		Yes - I would love a new connection	Yes;kbwhitney59@gmail.com;
14	2/15/21 11:41:48	2/15/21 11:42:53	5	Yes	No Preference;	Great to see this planning started. I think all options are good, with each having plusses and minuses.	Yes - I would love a new connection	Yes;andrew.f.moyer@gmail.com;
15	2/15/21 11:44:21	2/15/21 11:45:06	4	Yes	No Preference;	Sooner is better than later.	Yes - I would love a	Yes;dorer@tpsi.com;
16	2/15/21 12:25:06	2/15/21 12:28:56	4	Yes	Yellow Route;Black Route;	If the black or yellow route is chosen, why not develop the 'planned' Easton road path section and avoid adding the loop adjacent to the clover leaf?	Yes - I would love a new connection	Yes;ewbader@verizon.net;
17	2/15/21 12:30:08	2/15/21 12:30:52	5	Yes	No Preference;		Yes - I would love a	Yes;
18	2/15/21 12:35:29	2/15/21 12:38:20	4	Maybe (Explain in Question 4)	No Preference;	I'm curious where the potential trail access points for Doylestown Hunt would be located for the Red and Blue Route and if they will have an impact near my property in that neighborhood	Yes - I would love a new connection	Yes;golfcoursetesting@gmail.com;
19	2/15/21 13:04:52	2/15/21 13:05:34	5	Yes	No Preference;		No - I prefer my	
20	2/15/21 13:47:11	2/15/21 13:47:42	5	Yes	No Preference;		Yes - I would love a	Yes;
21	2/15/21 13:47:51	2/15/21 13:55:38	5	Yes	Black Route;Yellow Route;	The Red/Blue link down New Britain Road from Lower State Road provides a needed link from University Village in New Britain Borough to Central Park. The Black or Yellow Routes provide safe alternative to avoid conflicts with the golf course.	Yes - I would love a new connection	Yes;wvu95@yahoo.com;
22	2/15/21 14:35:40	2/15/21 14:36:21	5	Yes	Red Route;			
23	2/15/21 14:39:05	2/15/21 14:50:04	5	Yes	Yellow Route;	I like yellow as there is more buffer from 202 and golf course versus red/black. Yellow/blue crossing at 611/Easton is dangerous. Cars coming down the hill into town combined with right and left turns. Crossing lights are ineffective (see Lower State/ Wells for example). Need to consider a light? (not crazy about that idea though). How would 202/State Street intersection be reconfigured to protect cyclists? That is a launch point for cars heading S on 202. Might it be easier to create connection at East Street and cross over 202 there? Lastly, could clearly mark the Main Street overpass for cyclists so that access into borough is defined.	No - I prefer my neighborhood to stay separated	Yes;phale24@comcast.net;
24	2/15/21 15:02:57	2/15/21 15:10:02	5	Yes	No Preference;Yellow Route;	the yellow trail seems to link the homes further out which would be nice and folks in the borough can hook up from multiple routes no matter which route is selected. My area of concern is the Main St. bridge by the new Wawa and providing a clearly delineated path for folks to connect to the path. This will be a busy area and with more bikes could get a bit hairy!!		Yes;
25	2/15/21 15:48:46	2/15/21 15:49:25	5	Yes	Yellow Route;			Yes;Stephanieferraro@me.com;
26	2/15/21 16:03:39	2/15/21 16:04:04	5	Yes	No Preference;		No - I prefer my	No;
27	2/15/21 16:55:05	2/15/21 16:56:04	5	Yes	Black Route;		Yes - I would love a	Yes;Rkfelyd@gmail.com;
28	2/15/21 17:07:50	2/15/21 17:08:35	4	Yes	Red Route;			No;
29	2/15/21 16:27:07	2/15/21 17:16:02	5	Yes	Black Route;Black Route		Yes - I would love a	
30	2/15/21 18:28:31	2/15/21 18:31:54	3	Maybe (Explain in Question 4)	Yellow Route;Black Route;	I agree with keeping paved bike trails near or on roads where paving already exists. However I don't see why they have to be 10' wide... for instance Warden Road trail is only maybe 5' and in a place like that it is wide enough. It shouldn't be widened. The less paving that is done the better.... storm water etc.	No - I prefer my neighborhood to stay separated	Yes;madpie@verizon.net;

						<p>-As a former Septa Employee of the Railroad Division (Planning), (and as a sibling of a locomotive engineer), I strongly disagree with creating a pedestrian crossing along the railroad right of way due to numerous issues surrounding the heightened dangers associated with such a plan.</p> <p>- As a biker, I prefer to ride paths that do not parallel the bike/hike trail due to the excessive speeds, the amount of traffic and the lack of safe separation between cars and bikes.</p> <p>-The black trail appears to be the most distant from the 202 bike/hike path and eliminates the need to build the RR Crossing.</p> <p>-Bikers should wear helmets. If golf balls are going that far, are the cars driving on 202 also at risk of being hit? Fence sounds like it would solve that problem.</p> <p>-In response to number 5, I currently have access indirectly.</p> <p>-I am very interested in seeing completion of some of the other current dotted lines in Doylestown such as to fully connect N. Shady Retreat between Limekiln and Iron Hill Road and to provide access on Iron Hill on the other side of Ferry to connect pedestrians and bikers to Peace Valley Park.</p>		
31	2/15/21 18:15:11	2/15/21 18:47:04	3	Maybe (Explain in Question 4)	Black Route;	Thank you for the opportunity to provide input into this project.	Yes - I would love a new connection	Yes;for4millers@gmail.com;
32	2/15/21 20:30:19	2/15/21 20:45:03	4	Yes	Red Route;	<p>I have serious concerns about the proposed crossing of 202 to connect to lot at State St as well as the general distance between car traffic and this new section of trail (in entirety).</p> <p>Speed of traffic along this portion of 202 is significantly greater than the newer parkway portion.</p> <p>Crossing to State street would be a welcome opportunity for many reasons but one is kids getting to Fanny Chapman. I think that last crossing puts kids in a very risky crossing with cars heading southbound downhill along a gentle curve.</p> <p>Also the red trail is correct side for wawa and downtown main st access. I also am in favor of this based on the assumption the new trail is for bikes & pedestrians to share.</p> <p>Anyone who thinks the "lanes "(barely can call them that), along parkway marked for bicycles are actually safe for even an experienced cyclist to travel - needs to ride in those lanes once or twice and then reconsider if they'd want their loved ones out there.</p> <p>I have been jogging on the PATH when I've heard cars screeching and sliding off toward the "rail" and it's VERY disconcerting. (Approaching Lower State northbound was one location)</p> <p>This new section of the map cars currently travel! 50-70 +mph. Go stand there during morning snd evening rush hour snd see. Even assuming new speed limits will be posted and enforced - the path should be at a safer distance from the auto traffic.</p>		Yes;Npjj@verizon.net;
33	2/15/21 21:38:35	2/15/21 21:42:22	5	Yes	Blue Route;No Preference;	The expansion plans are wonderful for the community, families and individuals alike. Safe passage across busy roads or somewhat low sight areas is critical to keep everyone safe. Additional signage or warning mechanisms for vehicles may help.	Yes - I would love a new connection	No;
34	2/16/21 5:38:03	2/16/21 5:40:30	4	Yes	Red Route;		No - I prefer my	No;
35	2/16/21 7:18:58	2/16/21 7:24:30	5	Yes	Yellow Route;	This all looks great. We must remember to consider access to the trail. There are many neighborhoods along all routes which is good but navigating to the trail from those areas can be challenging. As an example, I can walk to the trail but need to walk along Pebble Hill Rd. As an adult, this is just a bit challenging. As a child (or adult with child) this borders on impossible to do safely. Currently, the option is to drive to a park and ride and quite frankly, this is an option that will keep us from using the trail much.	Yes - I would love a new connection	Yes;
36	2/16/21 8:39:25	2/16/21 8:41:20	4	Yes	Yellow Route;		Yes - I would love a	Yes;jsbalk@comcast.net;
37	2/16/21 8:44:05	2/16/21 8:44:46	4	Yes	Blue Route;Red Route;		No - I prefer my	No;
38	2/16/21 8:59:20	2/16/21 8:59:44	5	Yes	No Preference;		Yes - I would love a	Yes;
39	2/16/21 9:10:25	2/16/21 9:11:14	5	Yes	Red Route;	build as soon as possible!	Yes - I would love a	Yes;cstanford@mbakerintl.com;
40	2/16/21 9:09:23	2/16/21 9:14:26	5	Yes	Yellow Route;	Would love this - all plans look great	Yes - I would love a	No;
41	2/16/21 9:19:12	2/16/21 9:20:08	5	Yes	Black Route;			Yes;
42	2/16/21 9:21:23	2/16/21 9:22:06	5	Yes	Red Route;	I support this 100%!!	Yes - I would love a	Yes;
43	2/16/21 9:19:54	2/16/21 9:22:42	5	Yes	Yellow Route;		Yes - I would love a	Yes;
44	2/16/21 9:29:05	2/16/21 9:30:23	4	Yes	No Preference;	Would love an easy way to get from our neighborhood (Old Orchard) into town and to the 202 trail via bikes or walking. Love this idea!	Yes - I would love a new connection	Yes;
45	2/16/21 9:49:36	2/16/21 9:50:32	5	Yes	Red Route;		Yes - I would love a	Yes;timothy.elliott@yahoo.com;
53	2/16/21 9:53:22	2/16/21 9:55:22	5	Yes	Red Route;		Yes - I would love a	No;
109	2/16/21 13:31:39	2/16/21 13:47:44	5	Yes	Black Route;	Best to avoid surface level crossings at all costs, especially across busy, 4-lane highways.	Yes - I would love a new connection	Yes;halluskahandy@hotmail.com;
110	2/16/21 14:29:40	2/16/21 14:30:52		Yes	No Preference;	Nice idea		No;
111	2/16/21 14:57:44	2/16/21 14:58:43	5	Yes	No Preference;	More safe ways to get around town on a bike the better :)	Yes - I would love a	No;
112	2/16/21 15:08:31	2/16/21 15:09:16	5	Yes	No Preference;	I live in Doylestown hunt. Please build this	Yes - I would love a	Yes;
113	2/16/21 16:00:37	2/16/21 16:01:17	5	Yes	Yellow Route;		Yes - I would love a	Yes;
114	2/16/21 16:40:08	2/16/21 16:42:16	5	Yes	No Preference;	I love the idea of my boys being able to ride their bikes from our neighborhood (Old Orchard) to Doylestown Hunt on a dedicated, safe bike trail. I'd love to see more bikers out there and this is just the thing to get more people riding!	Yes - I would love a new connection	Yes;
115	2/16/21 17:11:51	2/16/21 17:15:22	5	Yes	Red Route;			Yes;
116	2/16/21 17:27:50	2/16/21 17:28:25		Yes	No Preference;		Yes - I would love a	Yes;
117	2/16/21 18:23:54	2/16/21 18:24:45	5	Yes	No Preference;	I think it's a terrific idea!	Yes - I would love a	Yes;
118	2/16/21 20:37:57	2/16/21 20:41:03	4	Yes	Red Route;	I would like to be able to access this trail from Green Street. It would be nice if the green street sidewalk went down to 202 and we could then get on the trail.	Yes - I would love a new connection	Yes;davidinscho@gmail.com;
119	2/16/21 20:47:00	2/16/21 20:58:30	4	Yes	Red Route;	As a Doylestown Hunt resident, I love the idea of access points from our neighborhood to the trails. This is such a large neighborhood with so many adults and children of all ages that could utilize the trails to safely walk or bike to Central Park, the Y, to schools like Linden and CB West, to Fanny Chapman pool, and other areas of town. This trail, really any of the 4 options, would be a fantastic addition to our Bike and Hike Trail system and to our community as a whole. If a decision is made to go with the black or yellow trail options, I'd like to see some improvements made from Steeplechase Drive/S. Main St intersection to the 202/Wawa intersection so that Doylestown Hunt residents can safely reach the trails near the Wawa.	Yes - I would love a new connection	Yes;amazant1@gmail.com;

120	2/16/21 21:16:54	2/16/21 21:19:05	5	Yes	Red Route;Blue Route;	I live in Doylestown Hunt and would definitely utilize the trails more if I could access it from the back of the neighborhood! Excellent idea!	Yes - I would love a new connection	Yes;amdidomenico@gmail.com;
121	2/16/21 21:46:13	2/16/21 21:48:19	5	Yes	No Preference;	Virtual meeting was very informative, but a video for less techie folks would probably help.	No - I prefer my neighborhood to stay	No;
122	2/17/21 2:16:08	2/17/21 2:22:56	4	Yes	No Preference;		No - I prefer my	Yes;Cakraussiii@comcast.net;
123	2/17/21 11:12:26	2/17/21 11:14:25	4	Yes	Black Route;Red Route;	Just very excited by the idea of connecting communities!		Yes;mkbingler@msn.com;
124	2/17/21 11:18:37	2/17/21 11:21:45	5	Yes	Black Route;	Terrific idea for our community. The presentation is excellent. It is a very worthwhile project that will benefit everyone.	Yes - I would love a new connection	Yes;william.h.loftus@gmail.com
125	2/17/21 11:28:40	2/17/21 11:32:25	4	Maybe (Explain in	No Preference;	Funding for the trail implementation and ongoing costs for trail maintenance, can the township manage this financially	Yes - I would love a new connection	Yes;
126	2/17/21 12:33:09	2/17/21 12:34:27	5	Yes	Yellow Route;	I feel like the yellow route creates a larger footprint with less obstacles and keeps people away from the bypass as much as possible		Yes;
127	2/17/21 12:53:29	2/17/21 12:57:08	5	Yes	Red Route;	This is a great way for families and individuals to enjoy the environment around them. Great mental and physical health environment.	No - I prefer my neighborhood to stay separated	No;
128	2/17/21 14:38:04	2/17/21 14:40:04	3	Yes	No Preference;	Love the intent of connecting the trails in the region. Dont have a preferred plan due to my location. Access will be enhanced with any one of the plans.	No - I prefer my neighborhood to stay separated	Yes;
129	2/18/21 10:56:10	2/18/21 11:01:37	5	Yes	Red Route;Black Route;	I'd like to see both the red and black routes completed. Offering alternative routes around the cloverleaf, depending on users' destination, and enhanced connection with several existing trails.	Yes - I would love a new connection	Yes;jasonforsell@gmail.com;
130	2/18/21 15:42:40	2/18/21 15:44:19	4	Yes	Blue Route;		No - I prefer my	Yes;
131	2/18/21 15:57:20	2/18/21 15:59:31	5	Yes	Black Route;	Please pave New Britain Road first, also we need a stop sign on/near New Britain Road near entrance to Doylestown Station.		Yes;bmault1775@gmail.com;
132	2/18/21 19:16:08	2/18/21 19:19:25	5	Yes	No Preference;	I STRONGLY encourage the planners to include fencing and trees where the trails pass through private residential areas, specifically along the Greens of Doylestown. We appreciate the proximity of a trail, but we appreciate the PRIVACY and SECURITY on our private property.	No - I prefer my neighborhood to stay separated	Yes;drjanice1@yahoo.com;
133	2/18/21 20:35:56	2/18/21 20:42:29	4	Maybe (Explain in Question 4)		I am all for bike and hike trails I think looping them is ideal at different points. So rather than end to end and opportunity to loop the trail at different points is very attractive. This would also allow other access points. The entire trail as well as segments in between. That is one of the challenges with Lake Galena it the whole loop (almost 6 miles of walking) or out and back the same trail. The maps on line here are hard to blow up large enough to see the exact locations, thus the 4 star.		Yes;dbradley90@gmail.com;
134	2/19/21 20:57:41	2/19/21 20:58:12	4	Yes	Red Route;		Yes - I would love a	No;
135	2/20/21 15:09:13	2/20/21 15:09:47	5	Yes	No Preference;			Yes;Lindsey.matthey@gmail.co
136	2/21/21 15:38:42	2/21/21 15:44:15	4	Yes	Red Route;Blue Route;	No strong preference as to which alignment, but would like the flexibility to get into Town and to Central Park	Yes - I would love a new connection	Yes;marylouparr@comcast.net
137	2/21/21 11:49:50	2/21/21 18:29:42	5	Maybe (Explain in Question 4)	Black Route;Blue Route;	Black or blue seem like the most direct (cost effective?) routes. I am opposed to the Blue trail. I was under the impression that that area was preserved land. The creek system that runs through that area floods during heavy storms, it is part of FEMA's flood plain. More flooding issues would be created by bringing in equipment and paving a portion of that area for the trail. It is a habitat to many wild animals including deer, fox, turkey, pheasants, turtles, snakes, owls and many other birds, also mosquito infested. Have planners walked that route? How many bridges and turnpikes would be necessary to construct that route to go over creeks and swampy areas? A suggestion would be to continue the Turk Rd. Trail ending near Bomicia Dr. to Pebble Hill Dr. then to Route 202. The opportunities/advantages would be: 1) A portion is already completed from Sauerman/New Britain Rd. to near Bomicia Dr. 2) Continuing from Bomicia Dr. to Pebble Hill Rd. would give students from North and South Woods of Pebble Hill developments attending Kutz Elementary School access for a safe walking and bike route to school. 3) Continuing route on Pebble Hill Rd. from Turk Rd. to Route 202 would provide a more convenient access for hikers and bikers from the Woods of Pebble Hill area to safely access borough for shopping and entertainment.	No - I prefer my neighborhood to stay separated	Yes;
138	2/21/21 22:11:09	2/21/21 22:11:50	5	Yes	Red Route;		Yes - I would love a	No;
139	2/22/21 10:49:02	2/22/21 11:20:15	4	Yes	Black Route;	I received in a letter from the Township showing a proposed Circuit Trail. A section of that trail between Sauerman Road and 202 goes along a creek. That area would be nothing but a money pit to keep maintained, it is a swamp and floods every time there is a heavy rain. Think about the Delaware Canal and the cost to repair that area when it floods. The Black trail as shown on maps is ok.	Yes - I would love a new connection	Yes;
140	2/22/21 12:47:07	2/22/21 12:49:03	4	Maybe (Explain in	I would prefer the opportunity for neighborhoods to connect	Maintenance cost over time and environmental impact - can we study how many people would use this path.		Yes;
141	2/23/21 10:32:06	2/23/21 10:34:34	4	Maybe (Explain in Question 4)	No Preference;	I live in Doylestown Hunt and back up to 202. Right now it's a nice wooded area that gives us privacy. If this is done would be good to have barrier/noise blocking built from 202 or plan to retain same amount of wooded space. My concern is road would become more visible abs louder and we'd lose privacy. There should be barrier wall present already once 202 was expanded	No - I prefer my neighborhood to stay separated	Yes;Sozioj123@gmail.com;
142	2/23/21 11:13:43	2/23/21 11:14:31	5	Yes	No Preference;		Yes - I would love a	Yes;rgpurdy19@gmail.com;
143	2/23/21 11:12:00	2/23/21 11:15:53	4	No	No Preference;	I see little value in any of the routes since they don't connect to anything at Poole's Corner. I use the trail system frequently for walking but I prefer money to be spent on something which enhances the trails as currently configured rather than a VERY long range plan to connect to New Hope.		Yes;slrock@att.net;
144	2/23/21 12:07:08	2/23/21 12:17:25	5	Yes		I think the ideal bike/hike trail is a separate one and not on the same road as vehicles, especially when biking with children. For example the one from Doylestown YMCA giving access to DelVal College and the Central Park woods from the 202 bypass. Would the 202-313 bike trail be a separate entity or a bike trail integrated into the road?		Yes;

145	2/23/21 12:41:21	2/23/21 12:43:17	4	Yes	Black Route;	This would be an excellent addition to the trail system and would allow residents on the east side of the borough to have a safe route to Central Park rather than having to cut through the city center with no protected bike lanes	Yes - I would love a new connection	Yes;	
146	2/23/21 13:41:55	2/23/21 13:43:53	3	Yes	Yellow Route;	No concerns, the sooner, the better!	Yes - I would love a new connection	Yes;stevennelson@comcast.net	
147	2/23/21 15:01:24	2/23/21 15:02:47	2	Maybe	(Explain in		Yes - I would love a new connection	Yes;J9Mitchell@gmail.com;	
148	2/23/21 15:11:15	2/23/21 15:13:51	4	Yes	No Preference;	Do not worry about errant golf balls. Let's get this project moving.	Yes - I would love a new connection	Yes;Cjstahl@gmail.com;	
149	2/23/21 17:34:01	2/23/21 17:38:36	4	Yes	No Preference;	Trails are essential so that we can all enjoy the areas beauty and amenities. I think the trails should be strongly pushed all the way to New Hope. I live off of 202 in between 313 and 413, and our community is locked in unless you walk or bike on 202 which is very dangerous. These trails would bring communities together and encourage physical activity in communities. People could actually bike to work or walk to town, which is great for individual health and cut down on car exhaust.	Yes - I would love a new connection	Yes;	
150	2/23/21 18:30:18	2/23/21 18:33:20	5	Yes	No Preference;	Yes please do this! My only concern are the crossings, especially black/yellow, when crossing Main St.	Yes - I would love a new connection	Yes;slaurilla@gmail.com;	
151	2/23/21 20:35:31	2/23/21 20:39:26	3	Maybe	(Explain in	Red Route;202 seems a bit excessive. Most bikers/walkers avoid being near highways unless it's known to be extremely safe.;	Should connect or collaborate with Buckingham township. They have great parks and would make for a great scenic trip with the family. Also, I think naming it 9/11 memorial trail is a bit much. The name of the trail should stay true to the culture of Doylestown or Bucks.	Yes - I would love a new connection	Yes;tuc41509@temple.edu;
152	2/23/21 22:46:17	2/23/21 22:49:24	5	Yes	Red Route;Yellow Route;	The proposed routes provide an essential link between existing trails and our community.	No - I prefer my neighborhood to stay	No;	
153	2/24/21 10:51:36	2/24/21 10:52:27	5	Yes	Blue Route;	This was very detailed and informative!	Yes - I would love a new connection	Yes;	
154	2/24/21 12:51:26	2/24/21 12:52:55	4	Yes	Blue Route;	Has there been any progress to connect Poole's Corner to New Hope along 202 besides just a line on a map?	Yes - I would love a new connection	Yes;	
155	2/24/21 14:19:25	2/24/21 14:20:54	5	Yes	Yellow Route;	Each has its merits. But personally I like being a little further from traffic and this seems to be the most cost effective	Yes - I would love a new connection	Yes;	
156	2/24/21 14:35:36	2/24/21 14:36:55	5	Yes	Black Route;	Black seems to be the path of least resistance and cost (and I have other very specific comments about location, traffic patterns, etc.)	Yes - I would love a new connection	Yes;bill@hogenauer.com;	
157	2/25/21 10:06:27	2/25/21 10:07:26	5	Yes	Red Route;		No - I prefer my	Yes;robert4solebury@gmail.co	
158	2/25/21 12:39:47	2/25/21 12:43:50	5	Yes	No Preference;	Thank you for the presentation. I'm a frequent rider on the Parkway and am thrilled about the idea of an extension. The Yellow route might be my favorite, but I hope if it's chosen it could include the other routes' options to make Easton, New Britain and Pebble Hill safer links to connect Doylestown to the route.	Yes - I would love a new connection	Yes;	
159	2/25/21 14:14:35	2/25/21 14:15:49	4	Yes	Red Route;Blue Route;Black Route;	Closer to Doylestown vs the other side of 202 might encourage users to stop at Wawa or other Doylestown businesses.		Yes;megan@frasers.org;	
160	2/25/21 14:24:29	2/25/21 14:26:18	4	Yes	Black Route;Yellow Route;	Make sense to meet up with an existing trail.	No - I prefer my	No;	
161	2/25/21 15:40:39	2/25/21 15:45:13	4	Yes	Blue Route;Red Route;	The trail terminates west of the Pools Corner intersection of 313 and 202 but should connect with the intersection. The alignment whichever is chosen should facilitate a connection to a potential corridor across Buckingham and Solebury to New Hope.		Yes;dacarzi@aol.com;	
162	2/25/21 17:18:31	2/25/21 17:18:52	5	Yes	Red Route;		No - I prefer my	No;	
163	2/25/21 18:39:36	2/25/21 18:42:01	5	Yes	Red Route;Blue Route;	I consistently use the existing trail for running and biking. This is a great expansion and hope any option can be completed quickly.	Yes - I would love a new connection	Yes;	
164	2/25/21 18:45:23	2/25/21 19:04:28		Yes	Red Route;	Red route could possibly pass under 611 overpass on south side of SEPTA tracks and then follow west side of 611 back to New Britain Road or could access New Britain Road at Lower State Road intersection thru property.	Yes - I would love a new connection	Yes;amarmel1@gmail.com;	
165	2/26/21 8:31:37	2/26/21 8:32:27	5	Yes	Yellow Route;		Yes - I would love a new connection	Yes;sean.f.maloney@gmail.com;	
166	2/26/21 8:30:41	2/26/21 8:32:49	5	Yes	No Preference;	I have full faith in the committee. This is an amazing virtual site, so well done. The route comparison was very helpful. All routes would add to our community.	Yes - I would love a new connection	Yes;Jillschweizer@verizon.net;	
167	2/26/21 8:41:29	2/26/21 8:44:30	5	Yes	No Preference;	As a cyclist, I think it is great that Doylestown is looking to expand the trail.	Yes - I would love a new connection		
168	2/26/21 9:14:35	2/26/21 9:20:42	3	Maybe	(Explain in	the new britain road section is the part that interests me.;	Yes - I would love a new connection	Yes;appbets@gmail.com;	
169	2/26/21 9:59:42	2/26/21 10:00:30	5	Yes	Yellow Route;				
170	2/26/21 10:25:57	2/26/21 10:28:26	5	Yes	No Preference;	As a regular user of existing trails in the area I think there is good cause for building these connections, I feel that the number of people I see using the trails warrants building the system further to promote healthy lifestyles and walk/bike commuting and travel.	Yes - I would love a new connection	Yes;erich.affett@gmail.com;	
171	2/26/21 10:43:50	2/26/21 10:44:26		Yes	No Preference;		Yes - I would love a new connection	Yes;	
172	2/26/21 11:04:09	2/26/21 11:05:05	5	Yes	Yellow Route;			No;	
173	2/26/21 11:08:38	2/26/21 11:10:29	4	Yes	No Preference;	Any expansion of safe, non-motored paths is a plus in my mind. Keep in mind, I answered "Yes" to question 5, but I realize my location up 313/611 is not feasible and doesn't make much sense right now.	Yes - I would love a new connection	Yes;tsheroff@gmail.com;	
174	2/26/21 11:58:54	2/26/21 12:00:31	5	Yes	Red Route;	I think this would be great if access from Clemens Farms neighborhood and Doylestown Hunt. Especially Clemens farms as we have no sidewalks and dangerous to walk/ride bikes for the kids in particular. Probably Meadow Lane access??	Yes - I would love a new connection	Yes;mbebb127@yahoo.com;	
175	2/26/21 12:17:01	2/26/21 12:19:38	4	Yes	Black Route;	I ride my bike on the Parkway trail frequently. I wish there weren't so many road crossings. Looking forward to a longer ride.	Yes - I would love a new connection	Yes;	
176	2/26/21 12:48:44	2/26/21 13:06:34	5	Yes	Yellow Route;	Just a comment on a possible relatively low cost way to enhance biking in the area. If you do a Google map search of "stable ct. and miller ave doylestown pa" the satellite view of the area shows a small section of "trail" that connects the two streets. That small path allows me to bike from home on mostly quite and low speed limit roads and eventually get on Fell rd. Buckingham township while spending very little time on Church School rd.. From there there are many routes one can take to stay on quite roads and explore the region. Cars don't like bikes slowing them down and I like to stay away from high speed cars as much as possible.	Yes - I would love a new connection	Yes;andyeverett57@gmail.com;	

					I ride often from New Britain to Doylestown, and work in Doylestown Borough, and this path would greatly improve my safety and options during rides. I like the connectors between Lower State and Central Park on the Red and Blue. This would enhance the walk/bikeability between Doylestown to Central Park in a way that will feel more meaningful for me. "Central" Park is anything but central, and it's difficult to get there without a car. Of all the options, the impact of the connector via New Britain road will be more easily felt since it connects directly to the popular 202 and lower State paths near the Y. Otherwise I selected Blue because the crossover near Easton rd gives us the option to take Main St safely to Warden Road, and this way connect back to existing trails in the lower section of Central Park, something I've done many times but rather hazardously without a crossing near the 202 ramps. So in essence it still gives the southern end of Doylestown improved access to the park. Everybody wins! :)	Yes - I would love a new connection	Yes;Gracerollins@gmail.com;	
177	2/26/21 17:57:30	2/26/21 18:15:10	5	Yes	Blue Route;			
178	2/26/21 18:39:06	2/26/21 18:40:21	5	No	No Preference;	Yes - I would love a	Yes;alaina9927@gmail.com;	
179	2/26/21 20:50:53	2/26/21 20:52:22	5	Yes	No Preference;	Yes - I would love a		
180	2/26/21 21:26:06	2/26/21 21:28:05	5	Yes	Red Route;	Yes - I would love a	Yes;	
181	2/26/21 21:26:33	2/26/21 21:41:25	4	Yes	Yellow Route;	No - I prefer my neighborhood to stay separated	No;	
182	2/27/21 7:44:00	2/27/21 7:53:46	5	Yes	Yellow Route;	Yes - I would love a new connection	Yes;larrygag@verizon.net;	
				Maybe	I prefer the yellow route but			
183	2/27/21 10:08:03	2/27/21 10:09:44	4	(Explain in	questions why the stretch	Why does the small section near Warden Rd. need to be widened?	Yes - I would love a new connection	Yes;mikemccormack18901@gmail.com;
184	2/27/21 10:36:21	2/27/21 10:38:37	4	Yes	Red Route;	Hope this all works out. Looks like a really nice improvement to the trail system.	Yes - I would love a new connection	Yes;raycartee@msn.com;
				Maybe		Cost is a concern since all routes involve what sound (to me) to be expensive modifications or new construction of bridges and other infrastructure. I would stay away from the golf course since bikers, walkers and golfers are not going to enjoy looking at a fence to contain errant golf balls.		
185	2/27/21 9:49:28	2/27/21 10:50:31	4	(Explain in	Question 4)	Yellow Route;	Yes - I would love a new connection	Yes;brenda.jones.bray@gmail.com;
186	2/27/21 11:42:58	2/27/21 11:47:14	5	Yes	Red Route;	Red trail seems to provide the most access for additional people. The black and yellow already somewhat exist on Warden Road. The red trail is nicer than the blue because it does not require an additional crossing at 202 at Poole's corner. Golf course should just provide netting. Red trail would allow access to the Y and Del Val and areas near that end of town.	Yes - I would love a new connection	Yes;
187	2/27/21 11:56:04	2/27/21 11:56:54	5	Yes	black or yellow;		Yes - I would love a	Yes;suzycat1@comcast.net;
188	2/27/21 12:44:17	2/27/21 12:50:12	5	Yes	Black Route;	Completing the New Britain road to Warren Road is great.... we have been wanting that link for years now, at least. A destination to my soon to be New WaWa will be Great for Everybody.	Yes - I would love a new connection	Yes;
189	2/27/21 14:15:54	2/27/21 14:24:17	5	Yes	Black Route;	I have used many of the trails, and really like their positioning, etc. The black trail keeps traffic from crossing Lower State Road twice, once where drivers will get more irritated because speed limit would likely be lowered to allow the bikes and people to cross the road, even though many many more cars travel than those using the trails. Black also crosses above the bypass route, allowing traffic at the East St light to now have to be slowed due to the crosswalk to allow bikers and walkers to cross. I also think that people walking and biking would prefer to not have to be put into harms way when possible, and would be prone to use the black trail more than the others. Also, the black/yellow trail doesn't have the hill altitude changes as much as the blue/red with New Britain and Lower State, thereby allowing for a good walk/ride without having to walk uphill for a long stretch. (A disadvantage to Peace Valley Park's trail if you are familiar with it.) In answer to #5, my neighborhood, off Shady Retreat Road, already has access to bike and hike, so I am not answering that question.		Yes;jonna.eiser@gmail.com;

190	2/27/21 14:14:55	2/27/21 15:49:26	3	Yes	No Preference;	<p>had a route that followed the creek (I believe it is called Town Creek) between Tower Hill Road and Oak Street from the 202 bypass to Turk Road. This alarmed us because it appeared to go right through our backyard (we live on Tower Hill Road) and through a wetland and flood plain. We were going to propose alternatives, which turn out to be the routes that are now listed as your alternatives, which we think are very good. So we favor your alternatives.</p> <p>In case the route between Tower Hill and Oak comes up again as an alternative, let me offer the reasons we think this creekside route is not a good alternative.</p> <p>1. You cannot simply lay a 10 foot wide asphalt path along this route. It is a wetland and floods regularly and significantly. There is no standing water most of the time, but the ground is so wet that only certain tree and bush species survive. When the location of the creek was moved to accommodate the 202 bypass and the sewer plant, it killed the major trees in the area because the ground is so wet. So it would need an elevated route or a bridge over most of the route. And this could not form a levee since that would alter the nature of the floodplain and likely cause flooding for residents on Oak and Tower Hill. The area along this creek regularly floods in any major thunderstorm and in a heavy rain it floods extensively across the entire floodplain towards Oak and Turk Road. Currently the extended floodplain provides protection against flooding for residents on both sides of the . Altering the topography increases the flood risk for residential properties, which would likely dramatically affect insurance rate. I can provide photos of past flooding of the area if you are</p>	Yes - I would love a new connection	Yes;
191	2/27/21 17:50:29	2/27/21 17:52:41	5	Yes	Yellow Route;	<p>I would love to be able to ride from Kids Castle park to Burpee park using the proposed yellow trail. Last summer I used the 202 parkway path alot. Great job on our Bike and Hike system. LOVE IT</p>	No - I prefer my neighborhood to stay separated	Yes;dianeshendler@gmail.com;
192	2/27/21 20:13:55	2/27/21 20:20:24	5	Yes	No Preference;	<p>I am well pleased that the future trail does not cross any wetlands! I think these proposed trails are better thought out than the initial one that impacted the wetlands behind Tower Hill Road. Thank you so much.</p>	No - I prefer my neighborhood to stay separated	Yes;laluebke@msn.com;
193	2/28/21 10:00:56	2/28/21 10:01:32	4	Yes	No Preference;	<p>Any trail at all will be an improvement on the current situation</p>		No;
194	2/28/21 11:30:09	2/28/21 11:30:42	5	Yes	No Preference;	<p>None</p>	No - I prefer my	No;
195	2/28/21 12:36:01	2/28/21 12:39:04	4	Yes	Along the 202 Parkway;	<p>A trail using the 202 Parkway is terrific. It should have been part of the State's planning when the Parkway was designed and built.</p>	Yes - I would love a new connection	Yes;
196	2/28/21 12:59:14	2/28/21 13:15:16	4	Maybe (Explain in Question 4)	<p>No Preference;Prefer access along existing trails from Central Park both along Wells rd. And along Neshaminy Creek out to intersection of Parkway and lower state road; then existing trail to cb west</p>	<p>To finish above, Ashland then merges with State st to connect with existing trail at east st. (I meant West st for turn previously.) Don't know logistics in terms of improvements to make this option work. Seems it could be accomplished by a combination of paint markings, barriers, or making some streets in Borough one way. Perceived advantages: link to Borough central business district as well as cultural amenities.</p>	Yes - I would love a new connection	Yes;Tclemons@clemonslaw.com
197	2/28/21 14:26:45	2/28/21 14:28:03	4	Yes	Black Route;	<p>I prefer the black route the most. The multiple road & rail crossings required from Blue/Red routes is less than ideal. Black seems to give the most scenic trail, with a more direct (and protected) route.</p>	Yes - I would love a new connection	Yes;uphigh@gmail.com;
198	2/28/21 15:34:25	2/28/21 15:35:12	5	Yes	Yellow Route;Blue Route;	<p>I'm very glad the proposed routes have been altered to avoid disturbing the wetlands and creek area.</p>	Yes - I would love a new connection	
199	2/28/21 15:52:31	2/28/21 16:04:48	4	Maybe (Explain in Question 4)	Black Route;	<p>I have significant concerns regarding both the planned trail on Pebble Hill Road and the trails on the south side of Route 202 as both of these have significant impact on the privacy of my property, create the potential for trespassing on my property and present potential liability with the pond. I would appreciate being put in touch with the appropriate person to discuss. I can be reached via email at sromanus@verizon.net. Thank you!</p>		Yes;
200	2/28/21 16:03:52	2/28/21 16:06:25	4	Yes	No Preference;	<p>appreciate our townships willingness to build and maintain these trails for use of the Residents and visitors</p>	Yes - I would love a new connection	Yes;tomnick85@yahoo.com;
201	2/28/21 16:01:01	2/28/21 16:52:43	4	Maybe (Explain in Question 4)	<p>I would prefer connecting the Spring Valley Road section of the Township into the existing Bike path system;</p>	<p>Currently, it is unsafe to walk or bike from the Spring Valley Road section of the Township to other neighborhoods and the existing bike paths. I have lived on Foxcroft Drive since 1993 and regularly walk and bike. In addition, this area is currently underserved, the closest bike path in the Township is 1.6 miles from my house. I think it would be better use of the Grant money to make short 'connecting paths' between neighborhoods. This would benefit far more kids and adults who would like to walk or bike to a friend's house or simply go on a safe walk or bike ride versus the proposed plan and would reduce the risk of residents being hit by automobiles. For example, constructing 'connecting paths' from: - Watercrest Drive to Hillcrest Drive along Spring Valley Rd and Pebble Hill Rd. - Windover Lane to Robyn Lane along Cherry Lane - Pebble Woods Drive to the bike path that ends at Bomaca Drive along Turk Rd. - the bike path that ends at Triangle Park along New Britain Road to connect to the bike paths in Central Park and the paths at Del Val. (at Lower State Road) The above locations are 'danger points' that I have observed on my own numerous bike rides over that past 28 years.</p>	Yes - I would love a new connection	Yes;
202	2/28/21 18:55:21	2/28/21 18:59:43	5	Yes	Yellow (first choice) or Black (second);	<p>Yellow seems the most direct. I ride many of those roads already. All are ok for experienced cyclists. The trail will be a big benefit for many average or casual cyclists. The Black alternative seems the next best to me. The link over to Wawa and the borough access is an advantage. I expect many cyclists will appreciate that.</p>		Yes;jttasala@gmail.com;
203	3/1/21 7:26:15	3/1/21 7:30:45	3	No		<p>As a property owner on Pebble Hill Road, I am not supportive of any trail that would run alongside my common driveway and property, which would compromise my privacy and security. I already have an issue with trespassers and nosey people entering my private property.</p>	No - I prefer my neighborhood to stay separated	Yes;rick@rickskidmore.com;

204	3/2/21 19:25:16	3/2/21 19:33:27	Maybe (Explain in 4 Question 4)	Black Route;	I would like clarification on how this study relates to the Circuit Trails study which is proposed to cut through the documented regulatory floodway that lies in between Tower Hill Rd and Turk Rd.	No - I prefer my neighborhood to stay separated	Yes;
205	3/30/21 9:21:31	3/30/21 9:27:30	4 Yes	Yellow Route;	I like the "Yellow" trail best as it would allow someone to directly link with the existing 202 Parkway Trail from the eastern side of Doylestown. The Red/Blue proposal looks like it would connect to the existing trail east of New Britain Rd, forcing trail users to cross New Britain Rd. to continue west. Over the past few years I have seen automotive traffic grow at the intersection of New Britain Rd. and Lower State Rd. and that could mean safety issues for trail users and potential conflict with drivers. The Yellow trail would avoid that crossing, connecting with the Parkway trail at Central Park/New Britain Rd.	Yes - I would love a new connection	Yes;
206	4/5/21 21:07:01	4/5/21 21:09:21	5 Yes	Yellow Route;	I like yellow route but still have a trail going up New Britain Road would be nice. Thanks	Yes - I would love a new connection	Yes;
207	4/30/21 10:07:12	4/30/21 10:14:08	5 Yes	Red Route;Blue Route;Is a hard copy of the the	It would be beneficial if the trail improvements included large canopy trees.	No - I prefer my neighborhood to stay	Yes;Jresek@aol.com;
208	6/3/21 15:15:51	6/3/21 15:25:47	5 Yes	Black Route;	Please, Please, PLEASE connect Turk Road with a pedestrian walkway as proposed!! This would provide our neighborhood a connection to the trail system. We are landlocked here with busy roads, that have little to no shoulder. (Bristol, Lower State, 611) Thank you for giving us this great platform to hear our voice!!	Yes - I would love a new connection	Yes;reuther.jim@gmail.com;

Thank you! - Tell us what you think.

143
Responses

08:37
Average time to complete

Active
Status

1. Rate the Virtual Public Meeting

 Insights

135
Responses

★ ★ ★ ★ ★
4.50 Average Rating

2. Are you supportive of this trail concept?

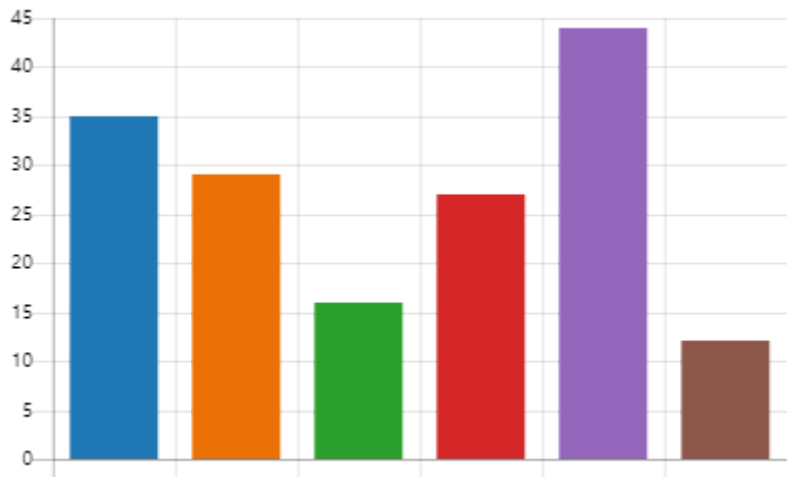
 Insights

- Yes 122
- No 4
- Maybe (Explain in Question 4) 17



3. Which Trail alternative do you prefer? Refer to Alternative Alignments Map as a reference

- Red Route 35
- Yellow Route 29
- Blue Route 16
- Black Route 27
- No Preference 44
- Other 12



4. Voice any comments you may have regarding the future trail location

💡 Insights

104
Responses

Latest Responses

"Please, Please, PLEASE connect Turk Road with a pedestrian walkway ..."
"It would be beneficial if the trail improvements included large canopy..."
"I like yellow route but still have a trail going up New Britain Road wo..."

5. Would you like us to explore an access point from your neighborhood to connect to this future trail?

💡 Insights

- Yes - I would love a new conn... 90
- No - I prefer my neighborhoo... 27



6. Would you like to be kept up to date in the progression of this project? (If yes, please make sure to provide an email address in "other")

- Yes 115
- No 21
- Other 69



Sample Easement Agreement

PREPARED BY AND RETURN TO:

PARCEL ID NOS.:

09-007-

09-007-

EASEMENT AGREEMENT

THIS EASEMENT AGREEMENT (this "Agreement") is made this _____ day of _____ **2012**, between The _____ (the "_____") and **DOYLESTOWN TOWNSHIP**, a political subdivision of the Commonwealth of Pennsylvania (the "Township").

BACKGROUND

A. The _____ is the owner of various parcels of ground situate in Doylestown Township, Bucks County, Pennsylvania (collectively, the "_____ Parcels"), as more particularly described on Exhibit "A" attached hereto and made a part hereof.

B. The _____ granted the Township an easement for a multi-use trail ("Existing Easement") through its property as identified on the _____ plan dated _____ and as approved by both the _____ and the Township on _____.

C. The Township desires to establish a multi-use trail (the "Trail") for use by the general public through and over the _____ Parcels and the _____ is willing to grant an easement to the Township for such purposes.

NOW, THEREFORE, the parties hereto, in consideration of the mutual promises contained herein and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, and intending to be legally bound hereby, covenant and agree as follows:

1. Incorporation of Recitals. The recitals set forth above and the exhibits attached hereto are hereby incorporated herein by reference as if set forth in full in the body of this Agreement.
2. The Existing Easement is hereby abandoned and all rights to it are forfeited by the Township.
3. Trail Easement. The _____, for itself and its successors and assigns, hereby grants to the Township and its successors and assigns, an easement benefiting the Township over, on, under and upon the _____ Parcels (the "Trail"), as outlined on the plot plan (the "Plot Plan") on Exhibit "B" and more particularly described on Exhibit "C", each attached hereto and

made a part hereof (the "Trail Area") for the purposes of constructing, inspecting, improving, operating, repairing, replacing and maintaining the Trail; provided, however, that the Trail Easement shall be under and subject to all conditions, restrictions, easements and agreements affecting the Trail Easement Area which now appear of record and to the following:

(a) Neither the _____ nor its successors or assigns shall erect or maintain any improvements on, over or under the Trail Easement Area which would affect access to or use of the Trail; provided, however, that the _____ shall be permitted to run utilities under or over the Trail, provided that the running of such utilities does not interfere with the safe operation of the Trail, and further provided that the _____ provides written notice of installation of such utilities to the Township, completes the installation in an expeditious manner, and restores the Trail to its pre-existing condition following installation of any utilities.

(b) The Trail Easement is not exclusive and the _____ and its successors and assigns reserve the right, after prior written notice to the Township, to grant such other easements, rights or privileges over, on, under and upon the Trail Easement Area to such other persons and entities as the _____ and its successors and assigns may elect, so long as such easements, rights or privileges do not interfere with either (i) access to or the use of the Trail, or (ii) the safe use of the Trail as a hiking and biking trail.

(c) If the Trail Easement Area traverses any private roadways owned by the _____, the _____ reserves the right to impose any traffic and safety rules and regulations with respect to such areas deemed necessary by the _____.

(d) The _____ shall have the right to close or limit access to the Trail Easement Area during such times when the _____ is performing operations that may pose a threat to health and safety including, without limitation, during times when the _____ is constructing or repairing facilities located on the _____ Parcels and other parcels owned by the _____ near the Trail Easement Area. The _____ shall only close or limit access to the Trail Easement Area for such periods of time as are necessary to remove any threat to health and safety and, during all periods of such closure or limited access, the _____ shall place conspicuous notice of such closure or limited access at the points where the Trail enters/exits the _____ Parcels.

3. Temporary Construction Easement. The _____ hereby grants to the Township a non-exclusive easement and right to use the portions of the _____ Parcels cross-hatched on the Plot Plan and more particularly described on Exhibit "D" attached hereto and made a part hereof (the "Temporary Construction Easement Areas") as may be reasonably required to provide temporary access and construction period storage and staging of

construction trailers, building materials and equipment and an access route for construction vehicles (the "Temporary Construction Easement"). The Temporary Construction Easement may be used only during and in connection with the initial construction of the Trail. This temporary easement right shall be exercised by the Township so as not to unreasonably interfere with the _____'s normal operations. The Township shall expeditiously complete any construction work that requires the exercise of this easement right and, upon completion of such work, promptly clean, repair and restore the Temporary Construction Easement Areas to substantially their condition that existed prior to Tenant's use of such Temporary Construction Easement Areas as reasonably required by the _____.

4. Construction Obligations and Covenants. Whenever pursuant to this Agreement the Township constructs any improvements in connection with the use and enjoyment of the easements granted herein, it is understood and agreed that such construction shall be performed in accordance with the following requirements:

(a) The _____ shall have the right to approve the plans and specifications for any improvements to be constructed within the Trail Easement Area, which approval shall not be unreasonably withheld, conditioned or delayed. If the _____ does not object to the plans and specifications within thirty (30) days after receipt, the plans and specifications shall be deemed approved.

(b) All such construction shall be performed in a good and workmanlike manner in accordance with sound engineering practice and in compliance with all applicable laws, ordinances, rules and regulations of all local, state and federal agencies and authorities having jurisdiction thereof, with the Township hereby agreeing to obtain prior to the commencement of such construction all permits, licenses and other approvals required in connection therewith from all governmental agencies and authorities having jurisdiction and to deliver copies thereof to the _____.

(c) All such improvements shall be constructed and completed free of any mechanic's or materialman's lien, and the Township hereby agrees (i) to indemnify and hold the _____ harmless from and against all loss, damage, cost, expense, action or cause of action arising from or in connection with any such mechanic's or materialman's lien, including, without limitation, attorney's fees and expenses and court costs, and (ii) to obtain and maintain the insurance required pursuant to Section 6 hereof.

(d) All construction of the Trail and any other improvements constructed by the Township hereunder shall be the responsibility of the Township and at the Township's sole cost and expense. Upon the commencement of the construction of the Trail by the Township, the Township shall proceed expeditiously to complete the construction of the Trail.

5. Maintenance Obligations and Covenants.

(a) The Township shall be responsible, at its sole cost and expense, for all repair, maintenance and replacement of the Trail, and any other improvements constructed in the Trail Easement Area by or on behalf of the Township, to the extent any such repair, maintenance or replacement is necessary to keep the Trail and any other such improvements in a safe and sightly condition. The Township shall also be responsible for the repair of any damage to the _____ Parcels resulting from the installation, operation, use, maintenance, repair, or replacement of the Trail and the Trail Easement Area.

(b) If the Township fails to perform its repair, maintenance and replacement obligations under this Section 5, the _____ may notify the Township in writing of such failure. In the further event the Township fails to remedy the defects set forth in the notice from the _____ within thirty (30) days after receipt of such notice, the _____ shall have the right, but not the obligation, to correct such deficiencies on behalf of and at the expense of the Township; provided, however, that in the event thirty (30) days is insufficient time in which to cure the default and (i) the Township is diligently pursuing such cure, (ii) the default can be cured within a reasonable period of time after expiration of the 30-day period, and (iii) the Township presents the _____ with a plan to cure such default which is reasonably satisfactory to the _____, the _____ will not pursue any remedies available to it. If the _____ performs any maintenance, repairs and replacements on behalf of the Township, the _____ shall have the right to bill the Township for the costs incurred by the _____.

(c) The insurance required pursuant to this Section 6 shall afford total protection to the limit of not less than Ten Million Dollars (\$10,000,000.00) with respect to injuries to persons (including death) with respect to each occurrence and Ten Million Dollars (\$10,000,000.00) with respect to damage to property.

6. Indemnification and Insurance.

(a) The Township agrees to indemnify and defend the _____ and to hold the _____ harmless from and against any loss, claim, damage, cost, expense, liability, action or cause of action, including, without limitation, reasonable attorneys' fees and expenses and court costs, arising out of or incurred in connection with the use of the Trail Easement or the Temporary Construction Easement by the Township and its agents, employees, contractors, servants, licensees and invitees (including, without limitation, members of the general public making use of the Trail Easement) unless the loss, claim, damage or liability is caused by the negligence or willful misconduct of the _____, it employees, agents, contractors, servants or licensees.

(b) From and after the date this Agreement is fully executed by the parties hereto, the Township shall carry general liability insurance on an occurrence basis against claims for injuries to persons (including death) and damage to property, occasioned by, or arising from, the existence, use and/or exercise of the Trail Easement and the Temporary Construction Easement and shall furnish certificate(s) of such insurance to the _____. Such insurance shall be maintained under a policy pursuant to which the _____ shall be named as an additional insured, or, alternatively, as a contractual insured.

(c) All of the insurance required to be maintained pursuant to this Section 6 shall be provided under a valid and enforceable policy of insurance issued by insurers licensed to do business in the Commonwealth of Pennsylvania and shall contain an agreement by such insurer to give at least ten (10) days' prior written notice to the _____ in the event of cancellation or change in the coverage or amount of insurance afforded thereunder; provided, however, that nothing contained in this Section 6 shall be deemed to prohibit the obtaining of policies of insurance which may cover any other property, premises, or liabilities of the Township, in addition to the coverage to be afforded pursuant to this Section 6.

(d) The Township shall provide the _____ with certificate(s) evidencing that all the insurance coverage required by this Section 6 is being carried on or before the date on which any such insurance (or renewals thereof) shall become effective.

7. Relocation of the Trail Easement. If either (a) the location of the Trail Easement Area, in the reasonable judgment of the _____, interferes with the _____'s operations and/or activities, or (b) the functioning of the Trail Easement Area is materially impacted by alterations and/or relocations of existing roads and driveways (and entrances and exits thereto) located on or adjacent to the _____ Parcels, then the _____ and the Township shall reasonably cooperate with each other to cause the Bike Hike Easement Area to be relocated in a manner satisfactory to address the foregoing concerns. If the Bike / Hike Easement Area is relocated pursuant to subsection (a), the _____ shall be responsible, at its sole cost and expense, for constructing any improvements necessitated by the relocation of the Trail Easement Area and if the Trail Easement Area is relocated pursuant to subsection (b), the Township shall be responsible, at its sole cost and expense, for constructing any improvements necessitated by the relocation of the Trail Easement Area. If the Trail Easement Area is ever relocated subsequent to the date of this Agreement, the parties hereto shall promptly execute and record an amendment to this Agreement identifying the relocated Trail Easement Area.

8. Termination of Easements. If, for any reason, construction of the Trail within the Trail Easement Area has not commenced by January 1, 2015, the Trail Easement and Temporary Construction Easement granted by the _____ hereunder shall automatically terminate and, following any such termination, if requested by the _____, the Township shall

promptly execute a writing, in recordable form, terminating such easements of record.

9. Notices. All notices, requests and consents herein required or permitted from either party to the other shall be in writing and shall be sent by nationally-recognized courier guaranteeing overnight delivery, or by mailing the same by registered or certified mail, postage prepaid, return receipt requested, at the addresses indicated below or to such other address as the party to receive same may designate by notice to the other. All such notices, requests and other communications shall be deemed to have been sufficiently given for all purposes on the day after the date of deposit with a courier guaranteeing overnight delivery, or if deposited in the United States mail, the date when the notice is either received or rejected by the addressee. All such notices shall be addressed as follows:

(a) If intended for the Township:
Doylestown Township
425 Wells Road Doylestown, PA 18901
Attn: Township Manager

(b) If intended for the _____:

or to such other addresses or entities as either party hereto may from time to time direct by notice to the other party hereto given as provided above.

10. Amendments. This Agreement may not be altered, modified, amended or terminated except by an instrument in writing duly executed by the parties then bound by this Agreement and in recordable form, which shall be recorded promptly and the expense thereof shall be borne equally by all parties then bound hereunder.

11. Headings. The headings used in this Agreement are for convenience and reference only and are not a part of this Agreement and do not in any way control, define, limit, or add to the terms, covenants, conditions and provisions hereof.

12. Governing Law. This Agreement shall be construed, interpreted and governed by the laws of the Commonwealth of Pennsylvania.

13. Binding Effect. This Agreement shall be binding upon and shall inure to the benefit of the parties hereto and their respective successors and assigns.

14. Covenants Running with the Land; Easements to be Private.

(a) It is intended that the covenants and agreements contained herein are to be covenants running with the land and affirmatively enforceable against the land and any grantee, successor and assign thereof.

(b) Notwithstanding the fact that it is understood that the Township will permit members of the public to use the Trail and the Trail Easement, neither the granting of any easement hereunder nor the use and enjoyment thereof pursuant to the provisions of this Agreement shall be deemed in any way to create or confer in or on any member of the public any estate or interest therein.

15. Entire Agreement. This Agreement contains the entire agreement among the parties hereto with respect to the subject matter hereof, and there are no other terms, conditions, provisions, understandings, statements, or representations, express or implied, concerning the subject matter hereof.

16. Counterparts. This Agreement may be executed any number of counterparts. All such counterparts shall be deemed to be originals and shall together constitute but one and the same instrument.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed, effective as of the day and year first above written.

Notary Public

TOWNSHIP:

DOYLESTOWN TOWNSHIP

By _____
Barbara Lyons Chairman of the Board
of Supervisors

COMMONWEALTH OF PENNSYLVANIA

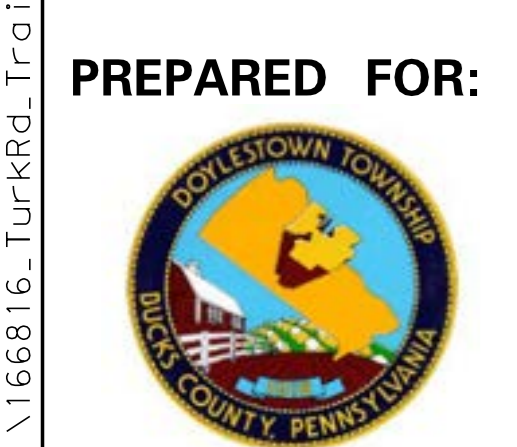
COUNTY OF BUCKS :

On this, the ____ day of _____ 2012, before me, the undersigned officer, personally appeared Barbara N. Lyons, who acknowledged herself to be the Chairman of the Board of Supervisors of Doylestown Township, a political subdivision of the Commonwealth of Pennsylvania, and that she as such Chairman, being authorized to do so, executed the foregoing instrument for the purposes therein contained by signing the name of the Township by herself as Chairman.

IN WITNESS WHEREOF, I hereunder set my hand and official seal.

. NOTARIAL SEAL

Implementation Plan/Construction Section Map



PREPARED FOR:
 PREPARED BY:
Michael Baker
 INTERNATIONAL
 500 OFFICE CENTER DRIVE SUITE 210
 FORT WASHINGTON, PA 19034

SR 202 (PARKWAY) TO SR 313 (SWAMP RD)
 TRAIL FEASIBILITY STUDY
 DOYLESTOWN TOWNSHIP, BUCKS COUNTY, PA

LEGEND:
 PREFERRED SHARED USE PATH/TRAIL

PLANNED TRAILS
 EXISTING TRAIL



E:\116816_1_TurkRd_Tr01_S Study\Plan\Map\Boroughs\RT-313\011313_Preferrd Route_sec1.tms2.dgn

Preliminary Cost Estimates



202 - 313 Trail Feasibility Study
Construction Section #1 (Central Park to Warden Rd Trail)
Prelim. Cost Estimate



Item	Description	Unit	Amount	Unit Price	Total Cost
Trail					
	Construction of ADA Concrete Curb Ramps	EACH	6	\$ 4,400.00	\$ 26,400.00
	Design of Concrete Curb Ramps	EACH	6	\$ 1,100.00	\$ 6,600.00
	Excavation, Class 1	CY	483	\$ 49.50	\$ 23,908.50
	Foreign Borrow	CY	1852	\$ 26.40	\$ 48,892.80
	Split Rail Fence	LF	1000	\$ 60.50	\$ 60,500.00
	Remove Existing Guide Rail	LF	1230	\$ 7.70	\$ 9,471.00
	Type 31-S Guide Rail	LF	1230	\$ 38.50	\$ 47,355.00
	Impact Attenuating Device	EACH	4	\$ 3,300.00	\$ 13,200.00
	Superpave Wearing Coarse, 1.5"	SY	2167	\$ 16.50	\$ 35,755.50
	Superpave Base Coarse, 3"	SY	2167	\$ 24.20	\$ 52,441.40
	SUBBASE 6" DEPTH (No. 2A)	SY	2167	\$ 16.50	\$ 35,755.50
	Asphalt Tack Coat	SY	2167	\$ 1.10	\$ 2,383.70
	Geotextile	SY	2167	\$ 2.75	\$ 5,959.25
				Total Trail	\$ 368,622.65
E&S/Drainage/PCSM					
	E&S	LS	1	\$ 10,000.00	\$ 10,000.00
	Drainage improvements	LS	1	\$ 20,000.00	\$ 20,000.00
	Stormwater Management	LS	1	\$ 40,000.00	\$ 40,000.00
				Total E&S/Drainage/PCSM	\$ 70,000.00
Traffic					
Signs					
	Signage (6 intersections)	LS	1	\$ 15,000.00	\$ 15,000.00
				Subtotal	\$ 15,000.00
Pavement Markings					
	Pavement Markings (6 intersections)	LS	1	\$ 25,000.00	\$ 25,000.00
				Subtotal	\$ 25,000.00
				Total Traffic	\$ 40,000.00
Misc.					
0201-0001	CLEARING AND GRUBBING	LS	1	\$ 30,000.00	\$ 30,000.00
0608-0001	MOBILIZATION	LS	1	\$ 28,717.36	\$ 28,717.36
	Design Survey / Base Map	LS	1	\$ 10,000.00	\$ 10,000.00
0686-0020	CONSTRUCTION SURVEYING, TYPE B	LS	1	\$ 10,000.00	\$ 10,000.00
0901-0001	MAINTENANCE AND PROTECTION OF TRAFFIC DURING CONSTRUCTION	LS	1	\$ 14,358.68	\$ 14,358.68
				Total Misc	\$ 93,076.04
				Subtotal Trail Construction	\$ 368,622.65
				Sub-Total E&S	\$ 70,000.00
				Sub-Total Traffic	\$ 40,000.00
				Sub-Total	\$ 478,622.65
				Sub-Total Misc	\$ 93,076.04
				Sub-Total	\$ 571,698.69
				Construction Contingency (5%)=	\$ 28,584.93
				Total Construction=	\$ 600,283.62
				Administrative Cost (2%)=	\$ 12,005.67
				Preliminary Engineering (15%)	\$ 90,042.54
				Environmental Studies (15%)	\$ 60,028.36
				Engineering and Inspection (10%)	\$ 60,028.36
				Total Project Cost=	\$ 762,360



202 - 313 Trail Feasibility Study
Section #2 (Warden Road Trail to Easton Road)
Preliminary Cost Estimate



Description	Item	Quan.	Unit	Unit Cost	Total Item Cost	
Paved Trail from New Britain Road to Easton Road	New trail (~3,635 ft)	Excavation (Shared Use Path)	1,199	CY	\$ 30	\$ 35,972
		6" Subbase	4,111	SY	\$ 15	\$ 61,667
		3" Bituminous Base Course	4,111	SY	\$ 25	\$ 102,778
		1.5" Bituminous Wearing Course (Shared Use Path)	4,111	SY	\$ 35	\$ 143,889
		Landscaping	3700	LF	\$ 10	\$ 37,000
	Intersection Improvement	Crosswalk (~36ft), etc.	2	Each	\$ 25,000	\$ 50,000
Misc.	Guiderail	100	LF	\$ 45	\$ 4,500	
	Impact Attenuating Devices	2	EACH	\$ 8,000	\$ 16,000	
	Right of way fence	3700	LF	\$ 18	\$ 66,600	
	ADA ramps	4	EACH	\$ 3,500	\$ 14,000	
res- Easton Road over	Barrier Separated Shared Use Path (Structure Mounted) with fencing on outside parapet	330	LF	\$ 1,500	\$ 495,000	
	Relocate Mountable Curb	503	LF	\$ 200	\$ 100,600	
Traffic	Intersection Improvement	1	Each	\$ 25,000	\$ 25,000	
Misc.	ADA ramps	6	EACH	\$ 3,500	\$ 21,000	
	Clearing and Grubbing	1	LS	\$ 10,000	\$ 10,000	
					\$ 1,184,006	

Signing / Pavement Marking (5%)	1	LS		\$ 59,200
Drainage/Stormwater (10%)	1	LS		\$ 118,401
E&S Control (5%)	1	LS		\$ 59,200
Survey (3%)	1	LS		\$ 35,520
Traffic Control (10%)	1	LS		\$ 118,401
			Subtotal =	\$ 1,574,727
			Subtotal with Contingency (20%)	\$ 1,889,673
Design (20%)				\$ 377,935
Construction Management & Inspection (10%)	1	LS		\$ 157,473
			Total =	\$ 2,110,135



202 - 313 Trail Feasibility Study
Section #3 - Easton Road to Pebble Hill Road
Preliminary Cost Estimate

Michael Baker
INTERNATIONAL

Description	Item	Quan.	Unit	Unit Cost	Total Item Cost
Intersection improvements (Easton Rd/On-Ramp)	ADA ramps	2	EACH	\$ 3,500	\$ 7,000
New trail alongside 202 - Easton Rd to Pebble Hill Rd.	Excavation (Shared Use Path)	972	CY	\$ 30	\$ 29,167
	6" Subbase	3,333	SY	\$ 15	\$ 50,000
	3" Bituminous Base Course	3,333	SY	\$ 25	\$ 83,333
	1.5" Bituminous Wearing Course (Shared Use Path)	3,333	SY	\$ 35	\$ 116,667
	Landscaping	3000	LF	\$ 5	\$ 15,000
New bridge	150 LF Contech Prefabricated Bridge Truss & abutments	1	Each	\$ 300,000	\$ 300,000
	Cast in place bridge deck	1	Each	\$ 14,000	\$ 14,000
	Bridge Abutments / Soil Testing	1	Each	\$ 100,000	\$ 100,000
Trail Connection from 202 trail to Pebble Hill Road	Excavation (Shared Use Path)	32	CY	\$ 30	\$ 972
	6" Subbase	111	SY	\$ 15	\$ 1,667
	3" Bituminous Base Course	111	SY	\$ 25	\$ 2,778
	1.5" Bituminous Wearing Course (Shared Use Path)	111	SY	\$ 35	\$ 3,889
	Landscaping	100	LF	\$ 5	\$ 500
subtotal					\$ 724,972
Misc	Right of Way Fence	3000	LF	\$ 18	\$ 54,000
	Guiderail updates	1	LS	\$ 100,000	\$ 100,000
	Clearing and Grubbing	1	LS	\$ 50,000	\$ 50,000
	Culvert Headwall Mod./Gabion Retaining Wall (Buck Run Creek)	1	LS	\$ 75,000	\$ 75,000
subtotal					\$ 928,972
subtotal with Contingency (20%)					\$ 1,114,766.67
	Signing / Pavement Marking (5%)	1	LS		\$ 55,738
	Drainage/Stormwater (15%)	1	LS		\$ 167,215
	E&S Control (2%)	1	LS		\$ 22,295
	Survey (3%)	1	LS		\$ 33,443
	Traffic Control (10%)	1	LS		\$ 111,477
				Subtotal =	\$ 1,504,935
	Design (20%)				\$ 300,987
	Construction Management & Inspection (10%)	1	LS		\$ 150,494
				Total =	\$ 1,956,416



202 - 313 Trail Feasibility Study
Section #4 - Pebble Hill Road to East State Street (Poole's Corner)
Preliminary Cost Estimate



Description	Item	Quan.	Unit	Unit Cost	Total Item Cost
Trail Connection Pebble Hill Road to East State Street (Poole's Corner)	Excavation (Shared Use Path)	1,491	CY	\$ 30	\$ 44,722
	6" Subbase	5,111	SY	\$ 15	\$ 76,667
	3" Bituminous Base Course	5,111	SY	\$ 25	\$ 127,778
	1.5" Bituminous Wearing Course (Shared Use Path)	5,111	SY	\$ 35	\$ 178,889
	Landscaping	4600	LF	\$ 5	\$ 23,000
				subtotal	\$ 451,056
Misc	Right of Way Fence	4600	LF	\$ 18	\$ 82,800
	Guiderail updates	1	LS	\$ 75,000	\$ 75,000
	Clearing and Grubbing	1	LS	\$ 50,000	\$ 50,000
	Barrier at East Road	1	LS	\$ 75,000	\$ 75,000
	Golf Course Netting (12' high)	2800	LF	\$ 20	\$ 56,000
					subtotal
	subtotal with Contingency (20%)				\$ 857,826.67
	Signing / Pavement Marking (5%)	1	LS		\$ 42,891
	Drainage/Stormwater (15%)	1	LS		\$ 128,674
	E&S Control (5%)	1	LS		\$ 42,891
	Survey (3%)	1	LS		\$ 25,735
	Traffic Control (10%)	1	LS		\$ 85,783
				Subtotal =	\$ 1,183,801
	Design (20%)				\$ 236,760
	Construction Management & Inspection (10%)	1	LS		\$ 118,380
				Total =	\$ 1,538,941